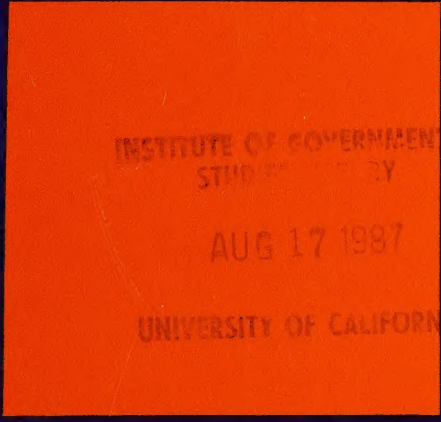


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# CASTRO SQUARE REVITALIZATION PLAN



INSTITUTE OF GOVERNMENTAL STUDIES  
AUG 17 1987  
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MOUNTAIN VIEW  
CALIFORNIA



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# CASTRO SQUARE REVITALIZATION PLAN

Prepared for

THE CITY OF MOUNTAIN VIEW, CALIFORNIA

By

SEDWAY/COOKE  
Urban and Environmental  
Planners and Designers  
San Francisco, California

With

GRUEN GRUEN + ASSOCIATES  
JOHN N. VOGLEY

November, 1974

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Albert C. Gribaldo  
Doni Hubbard  
Robert E. Parrett  
Jac M. Siegel  
H. I. Silversher

### **CITY PERSONNEL**

Richard DeLong - City Manager  
Robert S. Lawrence - Director of Planning

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Nash D'Angelo  
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Jim Levratto  
Raymond P. Meyer, Jr.  
Dale McKenzie

Alex McKinnon  
Emily Moehrke  
Gail Moore  
Mort Moore  
Robert Newson  
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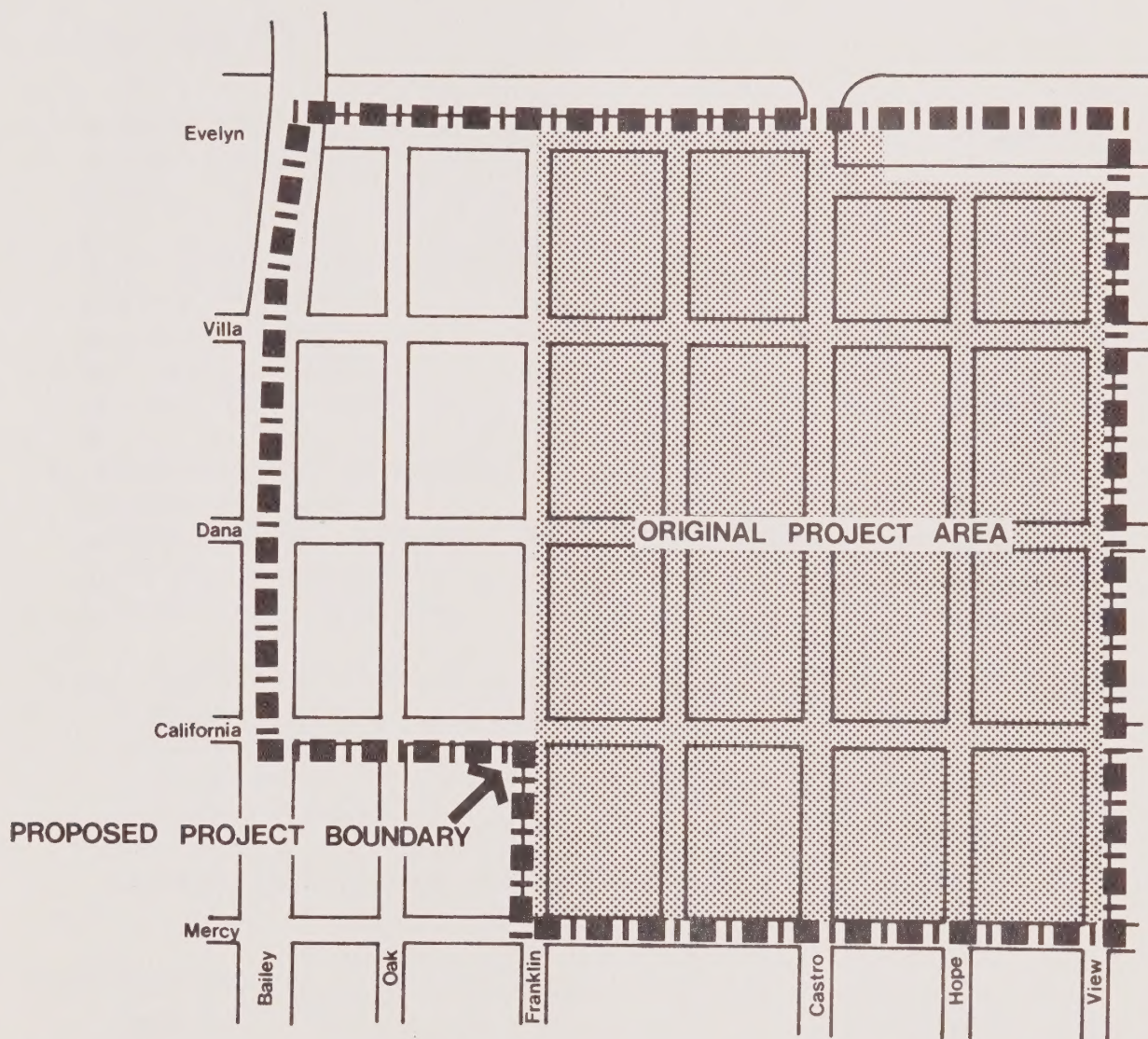
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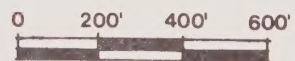
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**Figure 1**  
**REVISED PROJECT BOUNDARY**





# I. INTRODUCTION

On December 15, 1969, the Mountain View City Council adopted a revitalization plan for a 68-acre area then referred to as the Civic Center Shopping Area. (Since then the name of the area has been officially changed to Castro Square.) The plan encompassed an area generally bounded by Franklin Street, West Evelyn Avenue, View Street and Mercy Street. The Council's action prepared the way for financing major improvements in the area by means of tax increment financing provisions authorized in the State's Community Redevelopment Law. It also made available to the City other powers, including the right to acquire property necessary for implementation of the adopted plan.

The present plan, The Castro Square Revitalization Plan, represents a refinement of the original plan. Its purpose is to provide a more detailed basis for implementing the revitalization objectives through application of regulatory methods, and design and construction of major public improvements. Briefly, the Plan consists of three major components: 1) a land use element which identifies the type and location of permitted land uses; 2) a transportation element which sets forth provisions for auto circulation, public transit, parking, bicycle accommodations and pedestrian movements; and 3) an urban design element which specifies design conditions for both public and private improvements. Also included as a part of the Plan is an implementation program which establishes the regulatory methods to be used and outlines a schedule of improvements to be completed by 1982.

In the process of preparing the plan revision it has become apparent that a change in the original project boundary is desirable. It is proposed that the original 16-block area be extended to the west to include the area bounded by California Street, Bailey Avenue, the Central Expressway and Franklin Street, since this area is closely linked to Castro Square and affected by actions taken there. Excluding street rights-of-way this would add 16 acres to the project area. The proposed boundary and the original boundary are shown in Figure 1. Major auto, transit and pedestrian routes transect this western area. Moreover, like Castro Square, it is an older area which has begun to lose its original function as a single family neighborhood and consequently is in a state of decline. At the same time its location close to major existing and proposed transportation facilities and its proximity to both Castro Square and the Civic Center provide it with a residential potential which should not be disregarded. Inclusion of this area within the revitalization project boundary provides the means to ensure that the area can achieve its full potential and that development here will be coordinated with Castro Square improvements. In this way this area will benefit from Castro Square improvements and in turn enhance investment prospects in Castro Square by helping



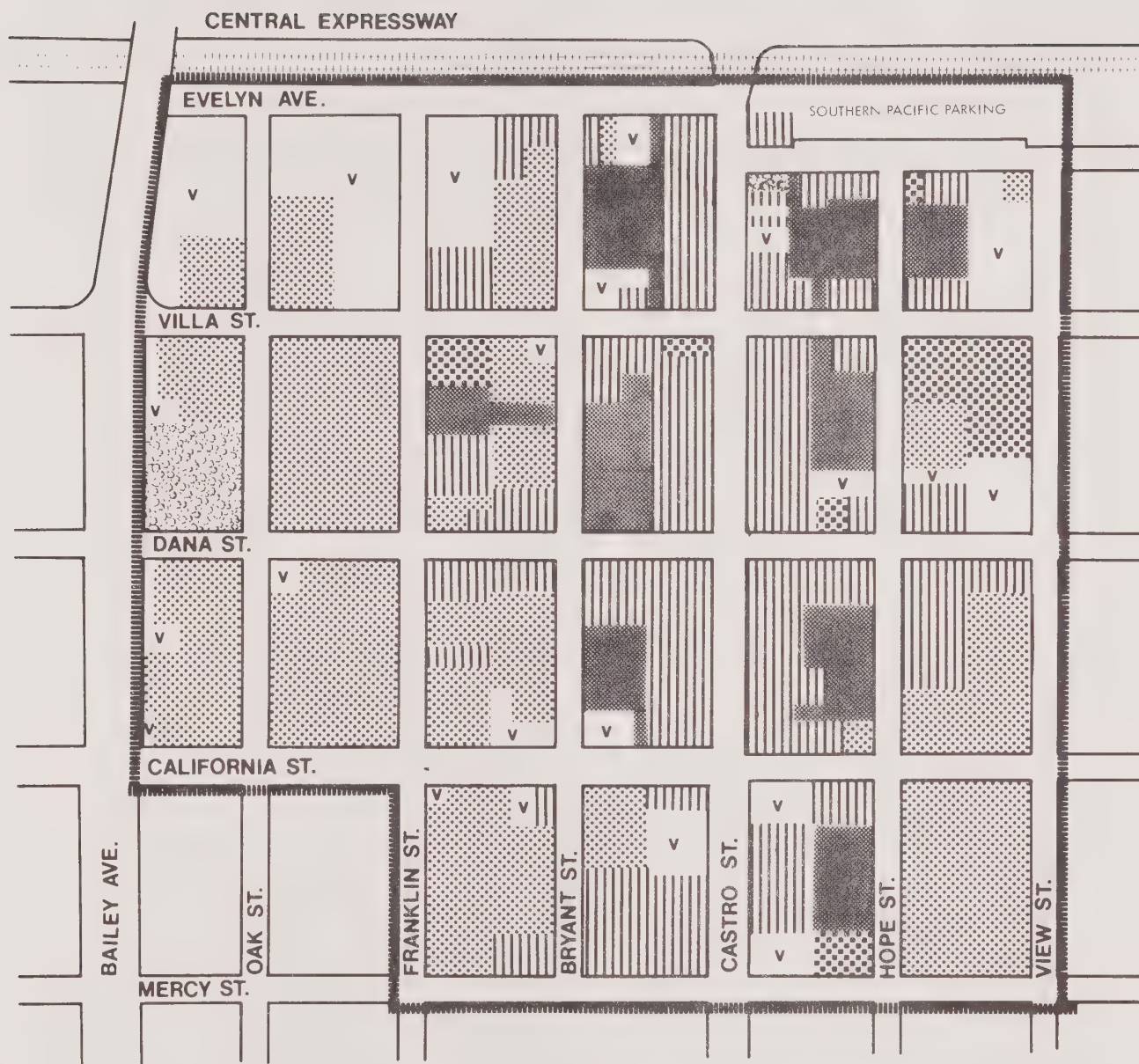
ensure stable conditions nearby. The plan provisions of Chapter 2 have therefore included this area. In order for these provisions to take effect it will be necessary to amend the present project boundary at the time plan amendments are approved.

## EXISTING CONDITIONS

### LAND USE

Existing land use conditions are more fully described in the previously published Castro Square Revitalization Program, Information Report No. 1. Major data findings of this report, based on a detailed land use survey of the original 16-block area in the Spring of 1973, are summarized below. Figure 2 shows generalized land use for the existing and proposed project area.

1. Exclusive of streets and other rights-of-way the total land area is approximately 50 acres of which 26 percent is devoted to residential uses, 26 percent to commercial uses and 22 percent to off-street non-residential parking. The remaining balance consists of vacant land and public or institutional uses.
2. While commercial land use in Castro Square consists of nine major land use categories, on the whole the economy is dominated by three use groups: 1) restaurants and food services; 2) banking, insurance, real estate, and related financial services; and 3) offices offering business, personal and professional services. Together these businesses account for 63 percent of the area's total commercial establishments and employ 700 people -- 72 percent of the area's total employment. Further reinforcing the dominance of these uses is the fact that they occupy almost half of the study area's total occupied commercial floor space of 557,000 square feet.
3. Vacant land and vacant building space is extensive throughout the area. Five of the area's fifty acres of land are vacant and there are about 274,000 square feet of vacant building space -- this includes the IED Building with 155,000 square feet of vacant space -- equivalent to 33 percent of the area's total commercial floor space.
4. Land utilization throughout the area is marginal and is indicative of the need for revitalization actions. Excluding lands owned by the City, other public agencies and utility companies, 33 percent of the land has improvements worth less than the value of the land, a clear indicator of underutilization and stagnation.



**Figure 2**  
**EXISTING GENERALIZED LAND USE**



Commercial



Vacant



Residential



Park



Educational, Religious,  
Public Service



Revitalization Project Area  
Boundary



Public Parking

0 200' 400' 600'





5. In the area west of Castro Square the land use is almost totally residential. In total there are 11.4 acres devoted to residential use, two acres of vacant private land and 2.6 acres in public ownership.

## TRANSPORTATION

Although Castro Square is centrally located within the City and close to both major city and regional roadways, access is limited by poor links with these facilities. Among the major problems is the lack of adequate connections between the Central Expressway and Bailey Avenue and the Stevens Creek Freeway and the Central Expressway. Because of these incomplete roadway connection, through traffic is unnecessarily mixed with Castro Square related auto and pedestrian movements.

At present, the total daily traffic volumes on Castro Street are 15,200 vehicles between California and Dana Streets, 15,500 vehicles between Villa Street and Evelyn Avenue and 22,200 vehicles between Evelyn Avenue and the Central Expressway. Other streets, such as Hope, Franklin and View Streets, in the Castro Square area are operating at traffic volumes well below their potential capacity. An October 1973 traffic analysis undertaken by De Leuw, Cather & Company, as a part of the revitalization study revealed that 60 percent or more of the vehicles using Castro Street entered but did not stop in the area. The major problem occurs at the intersection of Castro Street and Evelyn Avenue. At present Evelyn Avenue serves as a major east-west arterial collecting and distributing trips to residential areas in northeastern Mountain View and neighboring Sunnyvale. Because connections are poor to the Central Expressway from these neighborhoods, many motorists use Evelyn Avenue to gain access to the Expressway at Castro Street. In the analysis by De Leuw, Cather & Company, 22 percent of the traffic exiting the Central Expressway at Castro Street, and 16 percent entering here, is comprised of trips to or from Evelyn Avenue east of Calderon Avenue.

The Castro Square area has a total supply of 1546 public parking spaces. Of these, 759 are in public off-street lots and the remaining 787 are curbside spaces between Franklin and View Streets. On a project area basis present utilization of these parking spaces is low. However, in two areas, the block bounded by Villa Street, Castro Street, Evelyn Avenue and Bryant Street -- the block containing the main concentration of restaurants -- and in the three-block area bounded by Castro, Villa, Hope and Mercy streets, parking problems are evident. A major source of the problem is illegal all-day use of these spaces. Analysis also indicates that if businesses here were generating activity at levels experienced by many other sim-

ilar businesses elsewhere, parking shortages could be expected.

Although pedestrian access is provided by means of passageways from the parking lots to Castro Street and by mid-block crosswalks, little attention has been given to the needs of pedestrians. Sidewalks along Castro Street are narrow and existing street trees are more of an obstruction than an asset. Most of the passageways are drab and poorly lighted and the mid-block crossings are obscured from motorist view, thereby creating traffic safety problems. Moreover, pedestrian access from residential areas to the north is difficult due to on-grade crossings of the Central Expressway and the Southern Pacific railroad tracks. And although bicycle use has been rapidly increasing, provisions have yet to be made for accommodating this mode of transportation. A fuller discussion of these and other transportation conditions is provided in the De Leuw, Cather & Company report Castro Square Revitalization Area. Transportation Issues/Findings, issued in October, 1973.

## URBAN DESIGN

Major aspects contributing to the existing physical appearance of the revitalization area are its buildings, commercial signs, landscaping and public outdoor spaces such as streets and parking areas. These conditions were reported in depth in a preceding report, Castro Square Revitalization Program, Information Report No. 3. Physical Form and Appearance Options. Major urban design conditions documented in that report are as follows.

1. For the most part the existing architectural setting in Castro Square is undistinguished. (This condition was often mentioned in the opinion portion of the land use survey where frequent reference was made to the need to upgrade the physical appearance of buildings in the area.) Lack of exterior maintenance and nondescript and insensitive architectural treatment all contribute to the drab appearance. Most structures fail to related in color, materials or proportions to adjoining buildings. Moreover, haphazard "modernization" has often obscured the better, livelier elements of traditional architecture with a bland, nondescript facade so common on almost every main street in California.
2. Little attention has been given to the appearance of the rear of buildings along Castro Street, despite the fact this is often the main approach for many shoppers and business visitors. Rear entrances are mostly obscure or unattractive. As is the case in most older commercial areas, numerous sheds attached to the rear of buildings detract from the overall appearance of the area.

3. At present, the appearance of the area is marred by business signs whose size and position obscure building facades and thus visually dominate street views. In a large part, this is due to signs which project beyond building facades and above building roof lines. This also leads to conflicts between street landscaping and signs, the result being signs which cannot be read and visual disruption of the street landscape.

4. The lack of consistent landscape character and absence of well-established and maintained plant materials is another factor which undermines the viability of the Castro Square area. Existing vegetation in the area is generally limited to the outer blocks of the project area where the residue of earlier residential landscaping remains.

5. At present, public spaces in the project area consist primarily of the roadways, parking lots, and pedestrianways connecting Castro Street with the parking areas. Although these areas are of utmost importance since they provide the setting which should visually and functionally unify the Castro Square area, little attention has been given to their condition. Parking areas are only sparsely landscaped and poorly maintained; the passageways are drab, poorly lighted and lack visual interest; and the streets in the area have a deficiency of landscaping and are dominated by overhead utilities. Along Castro Street itself, the space given over to the automobile and the minimal area remaining for pedestrian use and landscaping make it difficult to achieve a pleasing appearance.

## SUMMARY OF PLAN

The major plan proposals are shown in Figure 3 along with a brief description of the proposals. The drawing illustrates the final phase of the program assuming construction of a regional transit station between Bailey Avenue and Castro Street. The Plan also includes a program for implementing these objectives. The key elements in the program are use of the Revitalization Plan as a "specific plan" as authorized by State law, and an eight-year, \$3 million capital improvement program.

Briefly, use of the specific plan approach will enable the City to guide private development actions in a manner superior to conventional zoning. The main advantage is that the specific plan can set use and design conditions to reflect specific site conditions. In contrast, zoning provisions must be uniformly applied throughout the City and thus are often inflexible. The specific plan would be administered by the City Planning Department staff in much the same manner as they now admin-



FIGURE 3

# PLAN SUMMARY



- ① RECONSTRUCT CASTRO STREET AS A SEMI-MALL PROVIDING FOR EXPANDED SIDEWALKS, REDUCTION OF AUTO TRAVEL LANES TO TWO LANES AND PROVISION OF BUS TURNOUTS AND A LIMITED NUMBER OF SHORT-TERM CONVENIENCE PARKING BAYS.
- ② DEVELOP A SERIES OF MINI-PLAZAS IN ORDER TO ENHANCE ACCESS FROM PARKING LOTS TO CASTRO STREET, PROVIDE FOR SMALL PROTECTED OUTDOOR AREAS FOR PEDESTRIAN USE AND ENABLE CONDUCT OF OUTDOOR COMMERCIAL ACTIVITIES.
- ③ REORGANIZE THE PRESENT AUTO CIRCULATION PATTERN BY CONVERTING FRANKLIN AND HOPE STREETS TO ONE-WAY ROADWAYS AND REDESIGNING THE HOPE AND CALIFORNIA STREETS AND CASTRO STREET AND EVELYN AVENUE INTERSECTIONS. RECONSTRUCT THESE STREETS AND PROVIDE FOR LANDSCAPING, UNDERGROUNDING OF UTILITIES, AND NEW STREET LIGHTING AND DIRECTIONAL SIGNING.
- ④ EXPAND PARKING ALONG BRYANT STREET AND MAKE PROVISIONS FOR CONSTRUCTION OF CIVIC CULTURAL FACILITIES.
- ⑤ DEVELOP MULTIPLE HOUSING IN CONJUNCTION WITH THE CULTURAL AND PARKING FACILITIES.
- ⑥ PROMOTE USE OF THIS AREA FOR LOW INTENSITY OFFICE USES COMBINED WITH RESIDENTIAL USES ALONG THE VIEW STREET FRONTAGE.
- ⑦ PERMIT INTENSIVE OFFICE DEVELOPMENT, INCLUDING PROVISIONS FOR EXPANDED PUBLIC ADMINISTRATIVE FACILITIES.
- ⑧ LOCATE REGIONAL TRANSIT STATION FACILITIES BETWEEN CASTRO AND FRANKLIN STREETS. PROVIDE FOR A PUBLIC PLAZA AND CONNECTING GRADE-SEPARATED PEDESTRIAN LINKS TO CASTRO SQUARE AND RESIDENTIAL AREAS NORTH OF THE EXPRESSWAY.
- ⑨ DEVELOP DANA STREET AS A MAIN PEDESTRIAN CONNECTION FROM ADJOINING NEIGHBORHOODS.
- ⑩ PERMIT CONSTRUCTION OF MULTIPLE HOUSING AND ENCOURAGE PRESERVATION OF SOUND EXISTING HOUSING.

## CASTRO SQUARE REVITALIZATION PLAN MOUNTAIN VIEW, CALIFORNIA

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0 100' 200'





ister zoning provisions in the project area. For purposes of administrative simplicity many of the City zoning provisions which are applicable are incorporated into the Revitalization Plan by reference. The City Planning Commission and the City Council would continue to exercise their present review and approval powers for development in the area. In this case, however, the Plan becomes the basis for review and approval processes.

The capital improvement program would be financed from five, and possibly six, major revenue sources. These sources are: 1) tax increments made possible by declaration of the area as a redevelopment project area in accordance with the State's Community Redevelopment Law; 2) revenues generated by an improvement district which was established at the time of adoption of the original revitalization plan; 3) annual assessments from the existing Parking District No. 2; 4) in lieu parking fees or lease payment generated by creation of a new Parking Deficiency District -- designed to prevent inequitable distribution of future parking costs to present business operators; 5) possible use of State Public Utility Commission utility undergrounding funds; and 6) Federal monies available to the City through the Community Development and Housing Act of 1974.





### ILLUSTRATIVE SKETCH

Castro Street improvements would provide for widened sidewalks, landscaping and reduction of the roadway to two traffic lanes.

## II. POLICIES AND PLAN ELEMENTS

The provisions of the Revitalization Plan for Castro Square are set forth in four elements: 1) the statement of overall policies; 2) the land uses to be permitted and conditions thereof; 3) the transportation facilities and services needed within the area and in other parts of the community; and 4) urban design requirements -- the way in which the physical accommodations needed by land use and transportation are sited and designed to achieve functional and aesthetic objectives. These last three elements should not be thought of as independent aspects which can be varied without regard to the other, but rather as completely intertwined elements which are separated out in this report solely for purposes of clarity. In brief, what is specified in this section is the type and location of activities to be carried on in the area, the accommodation of movements of people and goods between these and other activity locations, and the required physical facilities, i.e. buildings, roads, plazas and utilities.

### OVERALL POLICIES

The City's General Plan provides the broad policy framework within which these plan proposals have been conceived. The overall objectives below are basically a reconfirmation and amplification of the goals contained in the General Plan. The more specific policies contained in the land use, transportation and urban design elements serve as a means to promote these overall objectives. One major goal underlies all of the objectives and policies contained in this plan: CASTRO SQUARE SHOULD FUNCTION AS THE SOCIAL, GOVERNMENTAL AND CULTURAL HEART OF THE CITY. Historically Castro Street was the town center but this role has been eroded by a decline in the area's commercial function and deterioration of the immediate residential areas. At present the City administrative functions provide the major commitment to this goal. Each of the following objectives identifies ways of achieving this overall goal.

1. Uses which promote large day and night populations should be encouraged. Retail activities generating high levels of pedestrian activity should be encouraged and provision made for concentrations of office uses and higher density housing accommodations.
2. Transportation facilities should be improved to take better advantage of Castro Square's central location within the community. Improvements should: a) facilitate

better citywide access to Castro Square, with adequate provisions for bypassing of the area by auto traffic with other destinations; b) provide a focus for local and county transit service and a link with regional transit service; c) provide for pedestrian and bicycle movement and activities free from auto interference; and d) provide easy auto access to major Castro Square parking facilities without conflict with pedestrian movements.

3. Castro Square should visually express its role as the community's principal focal point and should convey a lively, urban atmosphere to those using the area.

## LAND USE ELEMENT

Figure 4 specifies the types and locations of permitted land uses within the Revitalization Area. The land use pattern shown has been selected to promote the major land use policies outlined below.

## MAJOR LAND USE POLICIES

1. The retail function should be retained and strengthened along Castro Street from Evelyn Avenue to California Street and priority given to uses which are pedestrian oriented. Emphasis should be given to retail uses which build upon the area's existing commercial resources. Specialty food firms, including uses such as restaurants, and stores selling produce, meats, seafood, poultry, dairy products, coffee, flowers and baked goods should be encouraged and accommodated along with home furnishing and improvement firms offering goods such as hardware, paints, unfinished furniture, nursery and garden supplies, tile, fireplace equipment, fabrics, floor coverings, home appliances and lighting fixtures. Firms providing complementary goods and services should be organized into groups providing shopper convenience while retaining the individuality of each firm.
2. Future City cultural facilities such as a community theater and a possibly relocated central library should be located in the Castro Square area. These facilities should be closely integrated with commercial and residential uses in order to maximize joint use of parking.
3. Provision should be made for office type uses which are both employment intensive and generators of frequent business trips.



FIGURE 4

# LAND USE POLICY DIAGRAM

PRIMARY PERMITTED LAND USES (SEE TEXT FOR  
OTHER USES CONDITIONALLY ALLOWED)

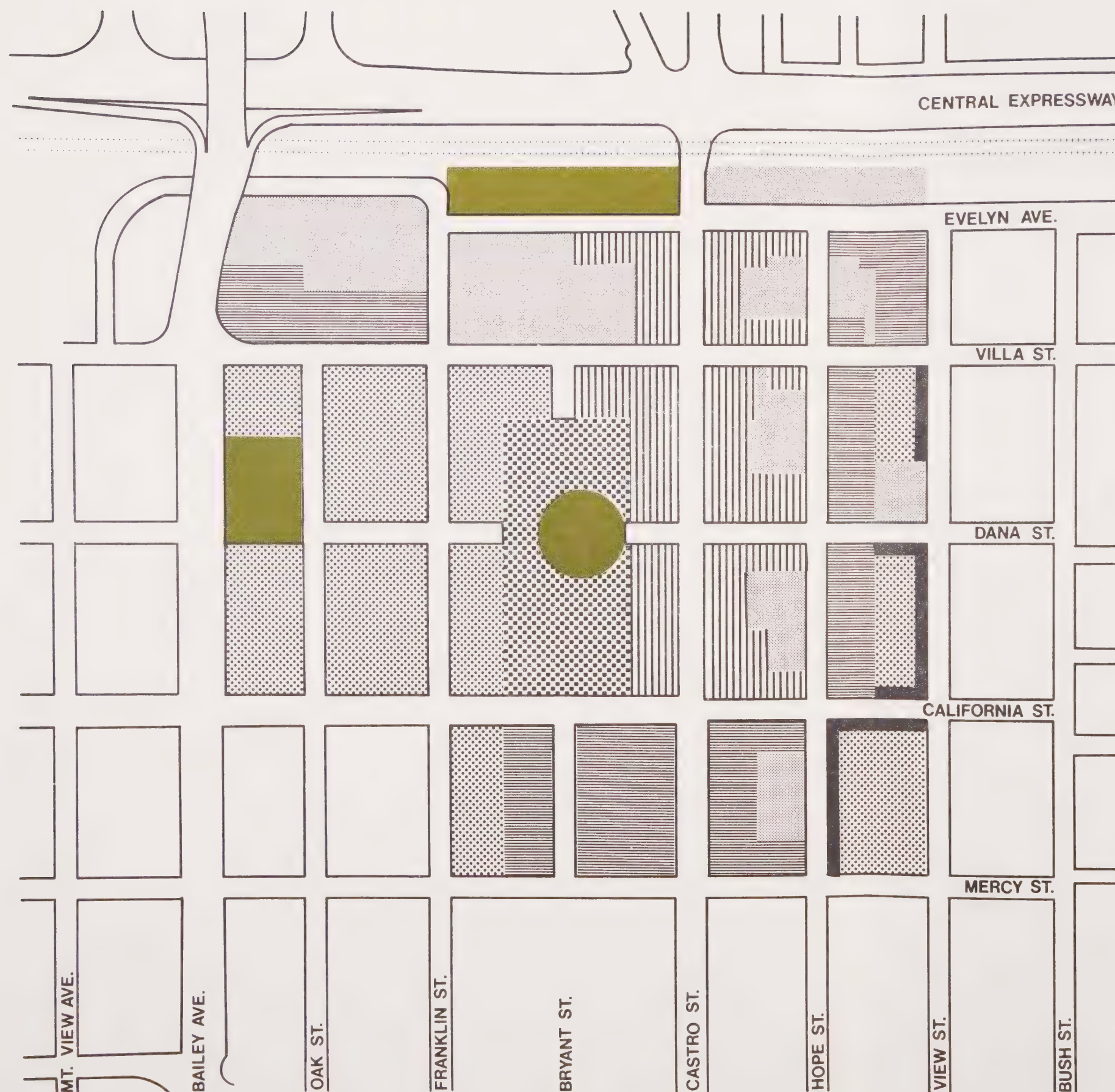
-  RETAIL
-  OFFICE/ADMINISTRATIVE
-  MULTIPLE HOUSING
-  PUBLIC PARKS OR PLAZAS
-  MULTI-USE AREA (CIVIC-  
CUTURAL FACILITIES, MULTIPLE  
HOUSING, AND PUBLIC PARKING)
-  OFFICE/ADMINISTRATIVE  
(GROUND LEVEL ONLY)
-  PUBLIC PARKING

## CASTRO SQUARE REVITALIZATION PLAN MOUNTAIN VIEW, CALIFORNIA

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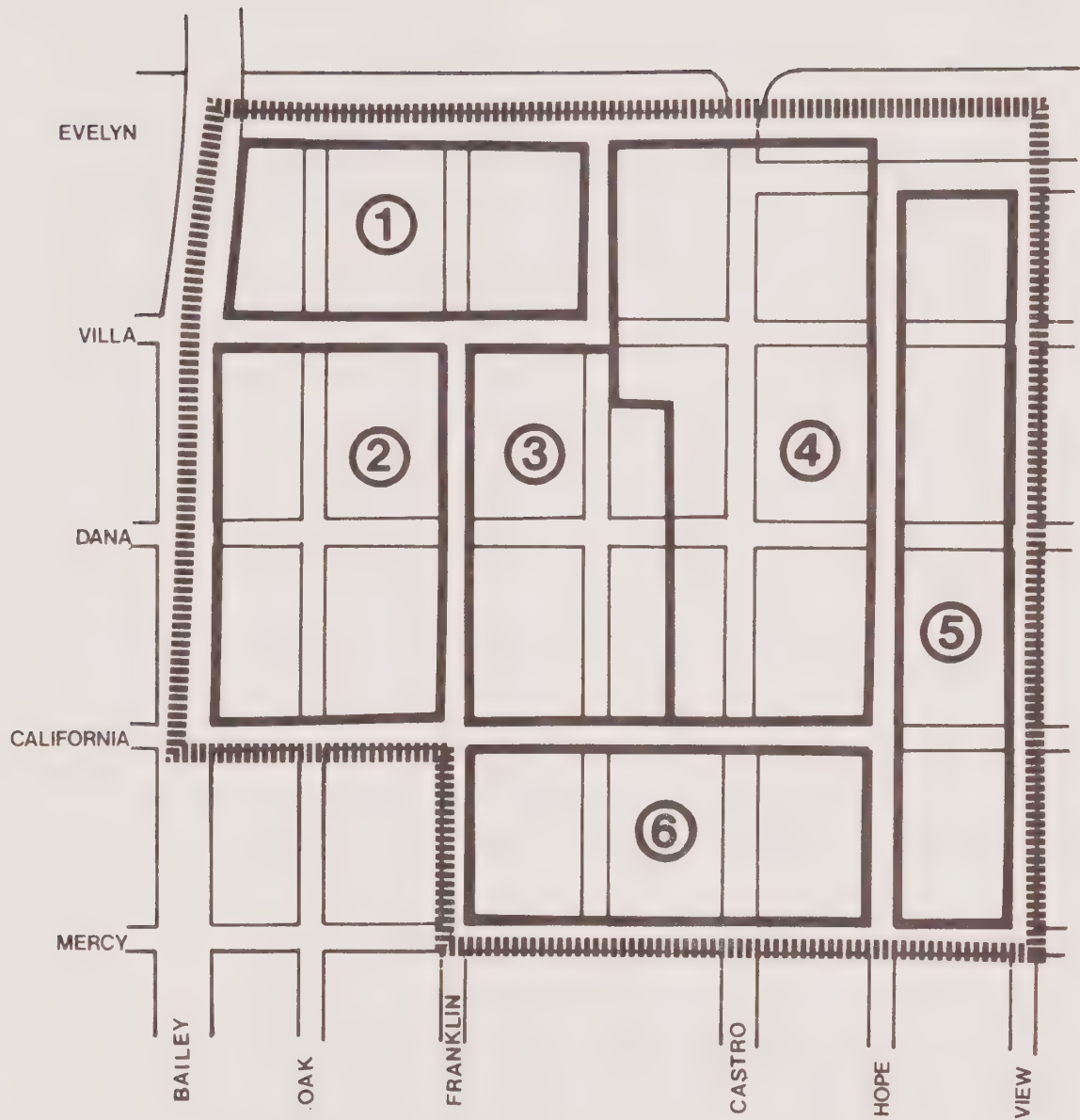
4. Multiple housing to meet the needs of residents seeking housing types other than single-family detached should be provided. This housing should be designed to accommodate the needs of older residents, the middle-aged who have passed the child rearing phase of life, and young married or unmarried adults. Castro Square housing should also serve people desirous of a more urban living pattern, and those seeking housing close to local and regional public transit in order to reduce their dependency on private automobiles.
5. Efforts should be made to promote better land utilization by: a) encouraging or requiring multiple use of land (for example, ground level commercial with upper floor residential use); b) requiring a minimum allowable intensity of use in addition to the conventional limits imposed by current zoning provisions; and c) development of multi-level parking facilities.
6. The single-family housing area to the east of Castro Square should be retained at approximately its existing density, and the family oriented environment enhanced. Adjoining land uses in Castro Square should be compatible with and complement this area.
7. Ample land should be devoted to the open space needs of Castro Square area employees, residents, shoppers and business visitors.

## SPECIFIC LAND USE REQUIREMENTS

The accompanying diagram identifies six subareas within the Castro Square Revitalization Area. Specific land use requirements for each subarea are set forth below. In each subarea existing zoning ordinance provisions are incorporated by reference. These provisions shall apply except where modified by conditions set forth in this Plan. The Urban Design Element section of the Plan should be consulted for additional requirements affecting land coverage, building heights, setbacks, open space and other design and siting conditions.

**CASTRO STREET.** The Castro Street frontage, from Evelyn to California Street, shall constitute the area's primary retail frontage. Special provisions are needed to protect this frontage from being preempted by non-retail uses which weaken and disperse retail concentration to the inconvenience of pedestrians. To avoid these problems, which often contribute to a decline of retail areas, an uninterrupted retail frontage should be maintained along this portion of Castro Street. Ground





**Figure 5**  
**SUBAREAS DIAGRAM**

- |                                 |                       |
|---------------------------------|-----------------------|
| ① Regional Transit Station Area | ④ Castro Street       |
| ② Oak Street Area               | ⑤ East Corridor       |
| ③ Dana Plaza                    | ⑥ Civic Office Center |

----- Revitalization Project Area Boundary

0 200' 400' 600'



level frontage along Castro Street should be limited to retail uses included as principal permitted uses in the Central Commercial District (CD) zoning provisions. All non-retail uses, such as banks, savings and loan offices and real estate offices shall be prohibited along the Castro Street frontage except for the following:

1) existing financial uses, real estate offices and theaters; 2) ground level access to upper floor commercial uses if limited to less than 15 feet of street frontage; 3) personal services such as barber shops, beauty parlors, and laundry and dry cleaning uses, conditioned upon demonstration that the uses are complementary to adjoining retail uses and the frontage occupied is not greater than 20 feet per establishment; and 4) theaters if occupying a frontage not greater than 20 feet.

The following uses shall be conditionally permitted: 1) outdoor restaurants; 2) any establishment providing entertainment; 3) establishments serving liquor, beer, wine or other alcoholic beverages for consumption on the premises; and 4) studios for photography, art, music, dance or other arts.

All other uses allowed in the CD District either as principal permitted uses, accessory uses, or conditional uses shall be permitted, with the exception of automobile service stations, automobile repair services, new or used car sales and trailer sales. These uses shall be subject to the following conditions: 1) they may not occupy building frontage within 25 feet of Castro Street; 2) the use and design of related facilities is consistent with policies established for secondary retail frontages; and 3) uses listed as conditional uses in the CD District shall continue to be conditionally allowed.

Residential uses shall be permitted if located above the first story. Residential uses shall be exempted from established parking requirements.

**DANA PLAZA.** The Dana Plaza subarea contains approximately 9.6 acres of land, one-third of which is publicly owned. Present uses are mixed and include 29 residences, the City police and fire facilities, commercial uses including an auto body shop and yard, scattered offices, vacant land, the Greyhound Bus Depot, and two City off-street parking facilities. Land utilization is low with more than 65 percent of private lands having improvements worth less than the value of the land.

This area should accommodate the following major land uses: 1) cultural uses such as a community theater, central library, public galleries, arts and crafts, instructional space, and other education-related facilities; 2) high density, urban types of housing; and 3) consolidated parking serving both public facilities and business

uses along Castro Street.

The R3-d High-density residential district provision of the City's zoning ordinance shall apply in the portions of this subarea designated residential unless modified by provisions of the Revitalization Plan. Within this subarea a mix of housing accommodations meeting the needs of the elderly, of young families, and others seeking an urban way of life should be provided. Commercial uses permitted in the Castro Street subarea shall be conditionally permitted here if limited to enclosed ground level space or second level space fronting on and with direct access to public walkways or plazas at that level. One parking space shall be provided for each dwelling. However, where units are specially designed and a long term commitment made to housing for elderly residents, the required parking may be reduced based upon determination of expected automobile ownership rates. All parking spaces shall be provided within structures. In no case may the number of parking spaces provided for residential use exceed the number of dwelling units provided. (The intent of this provision is to promote better land utilization, reduce dwelling unit cost, and attract residents who rely more heavily on public transit).

Land covered by parking shall be excluded for purposes of calculating lot coverage if the roof surface is designed, constructed and maintained as either public useable open space or useable open space serving the needs of residents of the building. This serves to encourage use of roof space for open space purposes.

**CIVIC OFFICE CENTER.** The three block area bounded by California, Hope, Mercy and Franklin Streets is designated as an office, administrative and professional, and business service area. The intent is to provide an area suitable for more intensive office use close to the retail uses of Castro Street and to the Civic Center. Both private offices and public administrative uses are appropriate in this area. The provisions of the Administrative and Professional Office District (O District) shall apply except for the western half of the block facing onto Franklin Street. Here the R3-d provisions shall govern. Within the residentially designated portion, permitted and accessory uses of the (O) District shall be conditionally permitted provided that the floor space devoted to such uses is contained within a residential structure and occupies no more than 20 percent of the total leaseable floor space of the building.

Within the area designated for commercial use, accessory uses listed in the (O) District shall be permitted uses and may have exterior identification signs subject to provisions set forth in the Urban Design Element of the Plan. Uses occupying



ground level space along Bryant and Castro Streets shall be limited to those which are functionally consistent with the existing and planned pedestrian use of these two streets and compatible with both the commercial uses to the north and Civic Center functions to the south.

Excluding enclosed space used for parking, a maximum floor area ratio (the ratio of building floor space to land area) of four shall be permitted in the block bounded by Franklin, California, Bryant, and Mercy Streets and on the east half of the block bounded by Castro, California, Hope and Mercy Streets. Within the remainder of the Civic Office Center area a floor area ratio of six shall be permitted.

**EAST CORRIDOR.** The East Corridor subarea contains the blocks bounded by Hope, Mercy, Evelyn and View Streets. It serves as a transitional area between low density residential areas to the east and the main commercial properties of Castro Square. Present uses are predominantly office and residential, interspersed with open parking lots.

This area should continue to serve this multi-purpose role but the intensity of both office and residential uses should be limited so as to maintain a compatible relationship to the adjoining residential neighborhood. The Land Use Policy Diagram Figure 4, specifies permitted land uses. Within the commercially designated portions the (O) District (Administrative and Professional Office) shall apply. Residential uses, subject to the provisions of the R3-d District, shall be conditionally permitted.

Within areas designated residential the provisions of the R3-d District shall apply. Uses permitted in the (O) District shall be conditionally permitted here if included as a secondary use in a residential structure and confined to the first story.

One parking space shall be required for each dwelling unit but in no case may the number of parking spaces provided exceed the number of dwelling units. All parking spaces shall be provided within a structure. Where open space use by residents is provided for on parking structure roofs, land covered by that portion of the structure shall not be included for purposes of calculating lot coverage.

**OAK STREET AREA.** The area bounded by California Street, Bailey Avenue, Villa Street and Franklin Street is designated as a high density residential district. The function of this area is to provide housing desirable for older residents, young married families, single persons, and other households seeking a more urban setting and an alternative to single-family living patterns. Housing in this area should

serve as an adjunct to Castro Square commercial areas with the latter providing most of the residents' daily commercial needs. Despite higher densities, provision should be made to ensure that at least 60 percent of all units are provided with direct access to and use of outdoor living areas. The provisions of the R3-d shall apply.

Required parking shall be reduced due to the proximity of planned regional transit facilities, provisions for good local transit, and nearness to commercial areas serving the needs of residents. One parking space per dwelling unit shall be required and the maximum number of parking spaces may not exceed the total number of dwelling units on the site. Where it can be demonstrated that the needs of residents do not require parking at the specified level, decreases up to 25 percent may be conditionally granted.

**REGIONAL TRANSIT STATION AREA.** Land contained within the area bounded by Villa Street, Bailey Avenue, Evelyn Avenue and Bryant Street is designated for use as a regional transit station area. Permitted uses shall include transit station facilities, parking for transit, other transportation related facilities necessary to promote a more convenient interchange between regional, county and local transit and retail and personal service uses intended primarily for the convenience of transit riders, and uses permitted in the (O) District. In the latter instance these uses shall be permitted in the area designated in Figure 4, with a permitted floor area ratio of three and a maximum ground coverage of 55 percent. The same uses shall be conditionally permitted in the balance of the subarea with a maximum floor area ratio of .8 and a maximum ground coverage of 40 percent.

## **CIRCULATION ELEMENT**

The Circulation Element provides for a coordinated system of auto, transit, bicycle and pedestrian routes to serve the needs of Castro Square and the City as a whole. These routes and related improvements along with parking and transit facilities are shown in Figure 6.

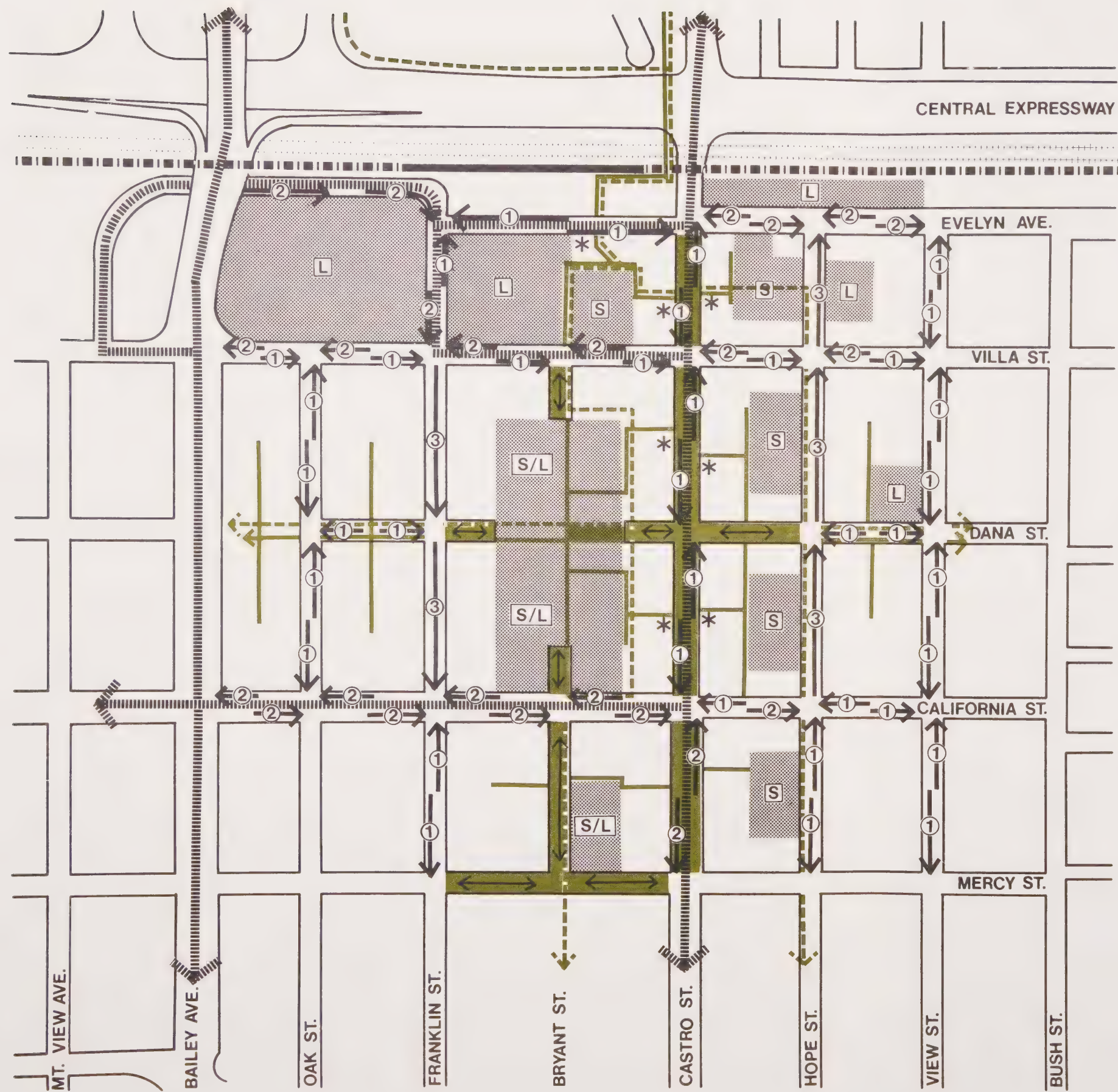
### **MAJOR CIRCULATION POLICIES**

1. Transit, auto, pedestrian and bicycle movements should be separated wherever practicable to promote a safe and efficient circulation system.



FIGURE 6

# CIRCULATION POLICY DIAGRAM



- REGIONAL TRANSIT
- COUNTY AND LOCAL TRANSIT
- TRANSIT BOARDING AREAS
- AUTO ROUTES
- TRAVEL DIRECTION
- NUMBER OF LANES
- RESTRICTED ROADWAY-LOCAL AUTO ACCESS ONLY
- PEDESTRIAN ROUTES
- BICYCLE ROUTES
- MAJOR OFFSTREET PARKING
- LONG TERM PARKING
- SHORT TERM PARKING

## CASTRO SQUARE REVITALIZATION PLAN MOUNTAIN VIEW, CALIFORNIA





2. Public transit should be developed to a level which provides an alternative to the use of the private auto for both work and non-work trips, such as for shopping and school. Moreover, transit should be given preference over private autos on the roadway where necessary for provision of fast, convenient service, and expansion of roadways for private auto use curtailed if such improvements encourage increased auto use at the expense of transit feasibility.
3. Vehicular access should be provided to Castro Square in a manner which is not disruptive to adjoining residential areas.
4. County wide and Regional access to Castro Square should be improved by facilitating more direct access from the Central Expressway, Stevens Creek Freeway, and U.S. 101.
5. The Castro Square area should function as the hub of the citywide transit system, with direct links between local and regional transit provided within the area.
6. A network of pedestrianways should be developed to link Castro Square with neighborhoods within convenient walking distances.
7. Since Castro Square is within convenient biking distance from most parts of the City, connecting bicycle routes should be provided between Castro Square and the various City neighborhoods.
8. Primary auto access to Castro Square uses should be provided for by a network of perimeter roads and related parking facilities, to reduce auto traffic on Castro Street.
9. Short-term shopping and business related parking needs should have priority over all-day employee parking. The latter should be limited to parking spaces at the outer edges of the area.
10. Additional parking should be provided in structures wherever possible to maintain convenient walking distances, to conserve land for other purposes, to avoid the visual disruption resulting from expansive parking lots and to minimize reductions in the tax revenues which result from underutilization of land.
11. Once a well-developed transit system incorporating local, county and regional services is in operation, the ratio of required parking to commercial space (retail shops, offices, personal service and business service establishments) should be reduced. This reduction will serve to both increase the use and feasibility of transit and promote

land utilization which recognizes decreased dependency on auto access.

## SPECIFIC CIRCULATION REQUIREMENTS

Figure 6 shows the major components of the Circulation Element. The requirements for each of these components are described below. Specific design requirements for these components are set forth in the Urban Design Element which follows.

**CASTRO STREET.** (California Street to Evelyn Avenue). This section of Castro Street shall jointly serve three major functions.

1. It shall serve as the main pedestrian circulation route for the Castro Square area. Sidewalk areas shall be expanded and driveways phased out so as to accommodate this function. Pedestrians shall be given priority over auto movements at all mid-block crosswalks and street intersections, with the exception of the California Street intersection.
2. Castro Street shall serve as a major local transit link between areas in Mountain View north and south of the Central Expressway and as a major route to future regional transit facilities. Bus turnouts shall be provided for in each block in locations convenient to the mid-block crossings. Sheltered waiting areas should be provided near the turnouts.
3. The roadway should serve as a local auto access route with provisions made to encourage diversion of through traffic movements. A limited number of curbside parking spaces may be provided to allow for short term parking and passenger pick-up and dropoff. However, these parking spaces shall be limited to ensure that they neither interfere with bus operations nor substantially reduce pedestrian space.

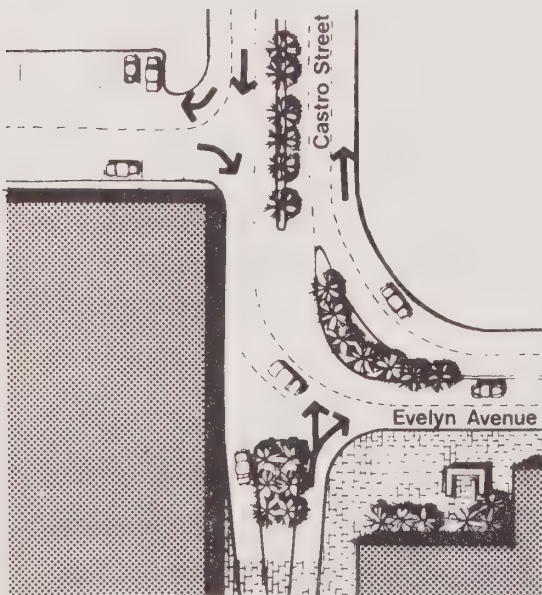
A roadway width of 28 feet, accommodating one moving lane in each direction, should be provided for vehicular use. A minimum sidewalk width of 14 feet shall be maintained along the street, increased to at least 22 feet at all end-block and mid-block pedestrian crossings. This latter width shall be maintained for a minimum distance of 25 feet on each side of a mid-block crosswalk and 25 feet on the inner block of end-block crossings.

A two-phased reconstruction of the Castro Street and Evelyn Avenue intersection is shown in the accompanying diagrams. In the initial phase of the Revitalization Pro-



gram, the existing Evelyn Avenue right-of-way would be maintained with two-way traffic permitted on the roadway both east and west of Castro Street. West-bound traffic on Evelyn Avenue shall be limited to right turns onto Castro Street. These measures are necessary to accommodate the heavy traffic movements on Evelyn Avenue, east of Castro Street, which are destined for either the Central Expressway or Moffett Boulevard, and will continue to be needed until such time as an alternative access route to the Central Expressway is provided for areas east of Castro Square. In the final phase, further modifications to this intersection will be needed if regional transit station facilities are constructed. A new alignment of Evelyn Avenue is proposed west of Castro Street to accommodate the transit station, with the road designed primarily as a busway serving the station area. Private vehicular use of this portion of Evelyn Avenue would be permissible if the road capacity is sufficient to accommodate both buses and autos. Left turns on Castro Street shall be prohibited for autos eastbound on this section of Evelyn Avenue but permitted for buses.

Initial Phase



Final Phase

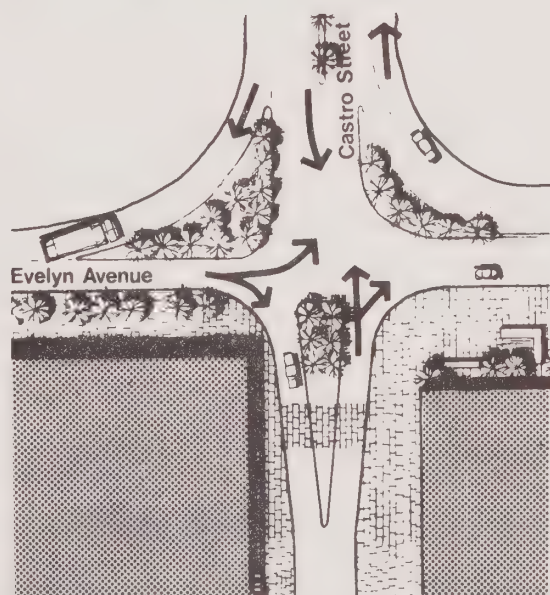


Figure 7

TRANSIT FACILITIES. Major recommended transit accommodations consist of County and local bus service operating on existing rights-of-way and regional and county transit accommodations employing transit vehicles operating on a fixed rail and grade separated right-of-way. The track and station facilities for the latter should be lo-

cated along the Southern Pacific Railroad right-of-way with the station area between Oak and Castro Streets. A City Council policy of maintaining an on-grade intersection of Castro Street and the Central Expressway requires the transit facilities to be either elevated or depressed within the Castro Square area. Since the cost of the depressed transit facility would be substantially higher than an elevated one, and since no major visual or noise problems are associated with the elevated facility, the latter is recommended.

Major routes for County and local bus service are shown in Figure 6. Castro Street and California Street would act as the major transit corridors in the area. The Plan permits transit vehicles to discharge and pick up passengers on Castro Street near the regional station or to turn on Evelyn Avenue and Villa Street and board and disembark passengers in the immediate station area. This will allow flexibility in future bus routing. Buses connecting areas north and south of the Expressway could operate in an uninterrupted manner along Castro Street or wait in the station area to permit better coordination of local and regional travel schedules. Routes having the station area as the terminal point would have ample provisions for discharging, picking up, waiting and turning about.

**PERIMETER ROADS.** The Circulation Policy Diagram depicts the perimeter road system and specifies the number of traffic lanes and their direction. Curbside parking would be prohibited along all of these streets. These roadways shall serve the dual function of channeling through-traffic around Castro Square and providing direct access to the major parking facilities encircling Castro Street. More specific design requirements are set forth in the Urban Design section of the Plan.

**PARKING FACILITIES.** At present there are 759 public off-street parking spaces provided within the Castro Square area. Although the total number of spaces provided is sufficient to meet current demands, problems occur due to heavier use in certain portions of Castro Square. Moreover, use of close-in parking spaces by employees in the area has created difficulties for short-term customer parking convenient to businesses. Reduction in the present number of parking spaces will also occur if present lots are redesigned to meet City standards and suitable landscaping provided. Thus it is necessary to both correct these deficiencies and make provisions for additional parking generated by new businesses. For example, if the IED Building does not use mechanical parking devices permitting the stacking of vehicles it will be necessary for the building owners to provide approximately 250 parking spaces in the immediate vicinity. To resolve present and future parking needs the following steps should be

taken.

1. Within the area bounded by Franklin Street, Evelyn Avenue, Hope Street and California Street all future parking needs shall be accommodated by public parking facilities. Private parking spaces shall be prohibited to avoid disruptive and wasteful land patterns and their accompanying visual problems.
2. Provisions for expanded parking shall be made by acquisition of additional properties along the west side of Bryant Street, plus several other parcels adjoining existing public lots along the east side of the street. In the initial phase of development this area should be constructed as a parking lot. This would provide an additional 300 parking spaces. In later phases, when related civic and residential uses are developed, the parking should be expanded further by construction of multi-level parking structures. Other provisions could be made for expanded parking along the east side of Castro Street by construction of multi-level parking facilities on present parking sites. (See Chapter 4 for a discussion of the methods of funding these improvements). Additionally, Dana Street, from Bryant to Hope Street, should be redesigned to accommodate diagonal parking. In the longer term future the Southern Pacific parking lot on Evelyn Avenue should be secured for municipal parking if no longer needed due to regional transit accommodations elsewhere.
3. In order to ensure adequate short-term parking for business firms, parking spaces between Bryant Street and Hope Street should be limited to one - and three-hour parking. The exact determination of the mix of one - and three-hour parking should be determined on a block-by-block basis in consultation with the business firms located in the block. This will permit consideration of the specific needs of these adjoining businesses.
4. All-day parking for employees should be provided by designating the City lot on the east side of Hope Street, between Evelyn Avenue and Villa Street, and the unimproved lot on the northwest corner of Dana and View Streets for this purpose. Approximately 100 cars can be accommodated. Additionally, a portion of the parking spaces provided in the area west of Bryant Street on the two sites should be designated for all-day parking. The number of all-day parking spaces should be limited to 35 spaces per 100 employees so as to encourage greater use of public transit. This figure is based on an objective of accommodating 50 percent of work trips by transit, walking or biking, and an average auto occupancy of 1.4 persons per car for the 50 percent of the work trips arriving by auto. At present there are approximately 1000 employees in the planning area. The City owned parcel on the east side of View Street, between Dana and Villa Streets, should not be used for parking since this would conflict with



the land use policy of maintaining the area east of View Street as a residential neighborhood. Therefore, the parcel should be declared surplus and sold for residential purposes and monies received used for acquisition of additional parking sites along Bryant Street.

5. For the convenience of business operators one reserve space could be permitted in the adjoining public parking facilities. Reserved spaces, however, should be restricted to spaces on the outer edge of the lot or structure.

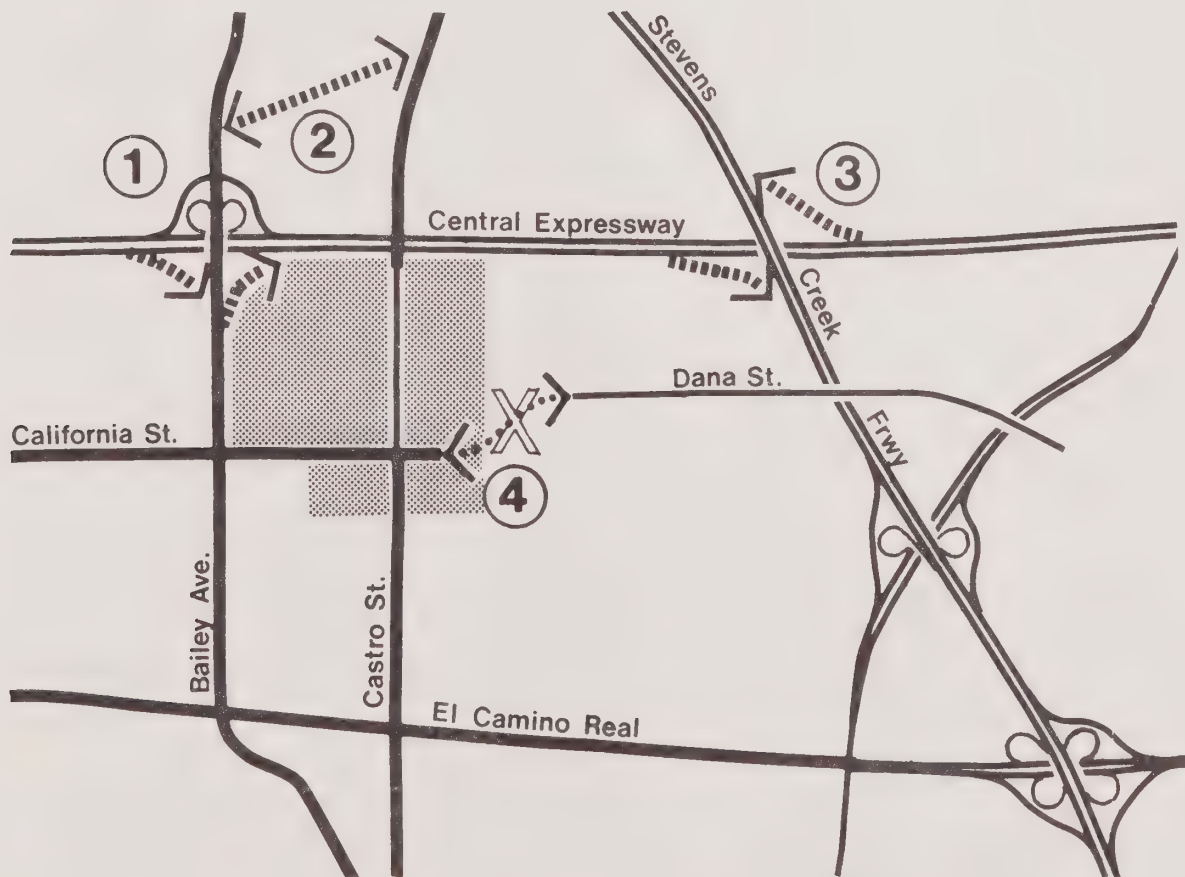
**PEDESTRIANWAYS.** A complete network of pedestrianways is proposed for the Castro Square area with links provided to neighboring areas. As already discussed Castro Street will serve as the main north-south pedestrian spine serving the commercial properties of Castro Street. Dana Street will serve in a similar role providing access to Castro Square from residential areas to the east and west. Along Dana Street shorter lateral pathways linking future residential development in the Oak Street area to Dana Street are also designated. Bryant Street serves as a secondary pedestrianway linking the existing civic center, proposed cultural area, expanded parking facilities, and the regional transit station facility. To improve access for residents north of the Central Expressway a pedestrian-bicycle overpass is proposed immediately to the west of Castro Street. This facility would connect to the regional transit station and provide grade-separated pedestrian access through the station area to Castro and Villa Streets.

**BIKEWAYS.** Until recently biking has been considered solely as a recreation activity, not as a means of transportation. Now, however, with increasing automobile congestion and accelerative operating costs for the private auto, bicycles are becoming an accepted means of transportation. Mountain View's gentle topography and land use pattern favor bike use. Better than 50,000 of the City's residents are within a two-mile radius of Castro Square, a distance which is considered reasonable for bike travel.

Special provisions for bicycle use are shown in Figure 6. These include a bikeway along Dana Street connecting Castro Square with residential areas to the east and west, routes along Hope and Bryant which connect with an extension of the bikeways accommodations on the Bailey Avenue Overpass, and a proposed pedestrian-bicycle overpass of the Central Expressway near Castro Street. The Bryant Street bikeway is proposed as a link with similar facilities in the Civic Center area.

Along these routes a separate pathway, nine to 10 feet in width, should be provided. Bike storage facilities should also be provided at the transit station area and along the rear of Castro Street. Use of bicycles on Castro Street should be discouraged due to conflicts with auto, bus and pedestrian movements.

**RELATED CIRCULATION PROGRAMS.** In addition to proposed circulation improvements within the Castro Square area, other roadway improvements are recommended to improve traffic conditions in central Mountain View. Elimination of several existing roadway proposals is also suggested. These recommendations are outlined below and shown in the accompanying map.



**NOTE: Numbers Refer To Text Items**

**Figure 8**  
**PROPOSED ROADWAY IMPROVEMENTS**  
**OUTSIDE THE PROJECT AREA**

1. The Bailey–Central Expressway Interchange should be completed to improve access to Castro Square and to permit diversion of through movements now traveling on Castro Street to Bailey Avenue.
2. A Moffett–Bailey connector should be provided to help divert through traffic from Castro Street to Bailey Avenue.
3. A Central Expressway–Stevens Creek Freeway connection should be provided in order to improve regional access to Castro Square and to reduce congestion problems at the Castro–Evelyn intersection. Approximately one-third of the vehicles presently using Evelyn Avenue immediately east of Castro Street would be diverted to the Expressway if this connection were provided.
4. A direct connection between California Street and Dana Street east of Castro Street should not be provided. Traffic analyses undertaken as part of the Revitalization Plan indicate no demand for this link. Moreover, encouraging through traffic in this area would have a disruptive effect on the residential neighborhood to the east of Castro Square and would interfere with the operation of the perimeter road system proposed around the main commercial area of Castro Square.
5. With Castro Street remaining open on grade at the Central Expressway it will be physically infeasible to construct the proposed Calderon overcrossing. Calderon Avenue overcrossing would also be physically precluded if a Central Expressway–Steven Creeks Freeway connection is provided. Moreover, financial feasibility is also doubtful since the Public Utility Commission funds providing 90 percent monies for grade separated railroad crossings would not be available if Castro Street remains open.
6. In order to reduce traffic congestion at the Castro–Evelyn intersection steps should be taken to discourage use of Evelyn Avenue as a through route for trips originating east of the Stevens Creek Freeway. This would serve to redirect a portion of the trips on Evelyn to the Central Expressway which has capacity to accommodate increased traffic volumes.

## **URBAN DESIGN ELEMENT**

The accompanying Urban Design Policy Diagram depicts the urban design requirements for the Castro Square area. This policy diagram, plus the major urban design policies outlined below and the specific urban design criteria which follow, provide



FIGURE 9

# URBAN DESIGN POLICY DIAGRAM



- UNINTERRUPTED BUILDING FACADE, UNIFORM BUILDING ALIGNMENT
- UNINTERRUPTED BUILDING FACADE, MINOR VARIATIONS IN SETBACK PERMISSIBLE
- REAR FACADES-SPECIAL DESIGN TREATMENT
- PARKS, PLAZAS, AND PEDESTRIAN WAYS
- PERIMETER ROADS-SPECIAL DESIGN TREATMENT
- DESIGN RESOURCE-BUILDING PRESERVATION RECOMMENDED

## BUILDING AREAS MAXIMUM HEIGHTS

- 40 FEET
- 80 FEET
- 140 FEET

## CASTRO SQUARE REVITALIZATION PLAN MOUNTAIN VIEW, CALIFORNIA

SEDWAY/COOKE  
URBAN AND ENVIRONMENTAL  
PLANNERS AND DESIGNERS

0 100' 200'







the means of promoting a functional and aesthetically compatible mix of land use, circulation and the physical environment.

## MAJOR URBAN DESIGN POLICIES

The basic urban design objective should be to develop an environment which functions well for the intended uses and provides persons living in or using the area with a satisfying experience. Expressing urban design aims is difficult since both of these aspects are in fact inseparable. The ability of an area to function effectively is dependent upon satisfying peoples' physiological and psychological needs while ensuring that autos move, shops receive their goods, and sufficient land or building area is provided for intended uses. The need for the proper functioning of business and other uses, and for accommodating the movements of people and material has already been articulated in the discussion of the land use and circulation elements. This must be complemented with 1) measures which achieve a strong visual identification for the area and define its special community role; 2) a setting scaled for use by both pedestrians and motorists; 3) an emphasis on an urban rather than suburban experience and; 4) an easily understood organization of the area and the relationship between major uses, parking, roads and transit accommodations. In varying degrees the following policies serve to promote these aims. These are detailed further under the discussion of specific subareas and components.

1. Building heights should vary in Castro Square, with taller structures permitted at major entrance ways to the area for emphasis and lower building heights maintained along areas with concentrated pedestrian activity.
2. Continuous, uninterrupted building frontages should be maintained in the main commercial area to promote a compact and convenient shopping pattern, to ensure a cohesive space interrelating the public area with adjoining private commercial uses, and to maintain visual interest for pedestrians.
3. A uniform design treatment should be used for each of the major circulation elements (major thoroughfare approaches, circumferential streets, pedestrianways, transit facilities and parking) so as to enhance the initial impression of Castro Square and promote a more readily understandable circulation system.
4. Pedestrian areas should be designed: a) to convey a sense of continuity between the various subareas of Castro Square; b) to retain a scale in keeping with pedes-



trian use, and c) to clearly distinguish the pedestrian priority of the space even where limited auto circulation may be permitted.

5. Along the commercially designated areas every effort should be made to integrate residences, cultural facilities, commercial space, and parking facilities rather than have each of these uses visually and physically isolated from each other.
6. No attempt should be made to establish a thematic concept based on historic or exotic architectural styles. Instead, a policy of guided diversification should be followed with the integration of successive elements dependent upon use of common building materials, consistent landscaping and application of general rules ensuring consistency in the height, bulk and siting of individual buildings. The individuality of each building should continue to be stressed, but not to the extent that it represents a discordant element in the larger setting.
7. Building materials consistent with an informal yet urban place should be required. These include brick, wood (if not employed in a rustic manner), and concrete if provided with a tasteful texture. Glass and steel are also appropriate, if complemented by scale providing details and colorful accessories. Predominant colors should be limited to those commonly associated with the material employed, with accents provided by primary colors in such accessories as awnings and signs.
8. Trees, groundcover, and paving materials should be limited to several types and selected to help convey the sense of an urban place. They should also serve as a primary means of unifying the appearance of the area, thereby permitting greater diversification in the design of individual buildings. Within selected areas, a lush garden-like environment could be appropriate as a focus for activities such as restaurants.
9. Signs should be designed to promote an informal yet quality environment consistent with intended commercial roles and the Civic Center. Signs, therefore, should be subordinated to buildings and public areas. This means signs should not project above the roof line of buildings and should be designed as an integral part of the buildings -- not as tacked-on, unrelated elements. As well, there should be a consistency in size, color and placement of signs to promote a colorful, attractive appearance without creating a feeling of chaos.

## SPECIFIC URBAN DESIGN POLICIES

**CASTRO STREET.** The design scheme for Castro Street, from California Street to Evelyn Avenue, is shown in Figure 10, and is further illustrated in the street elevations shown in Figure 11. The following specifications should be observed in the final design.

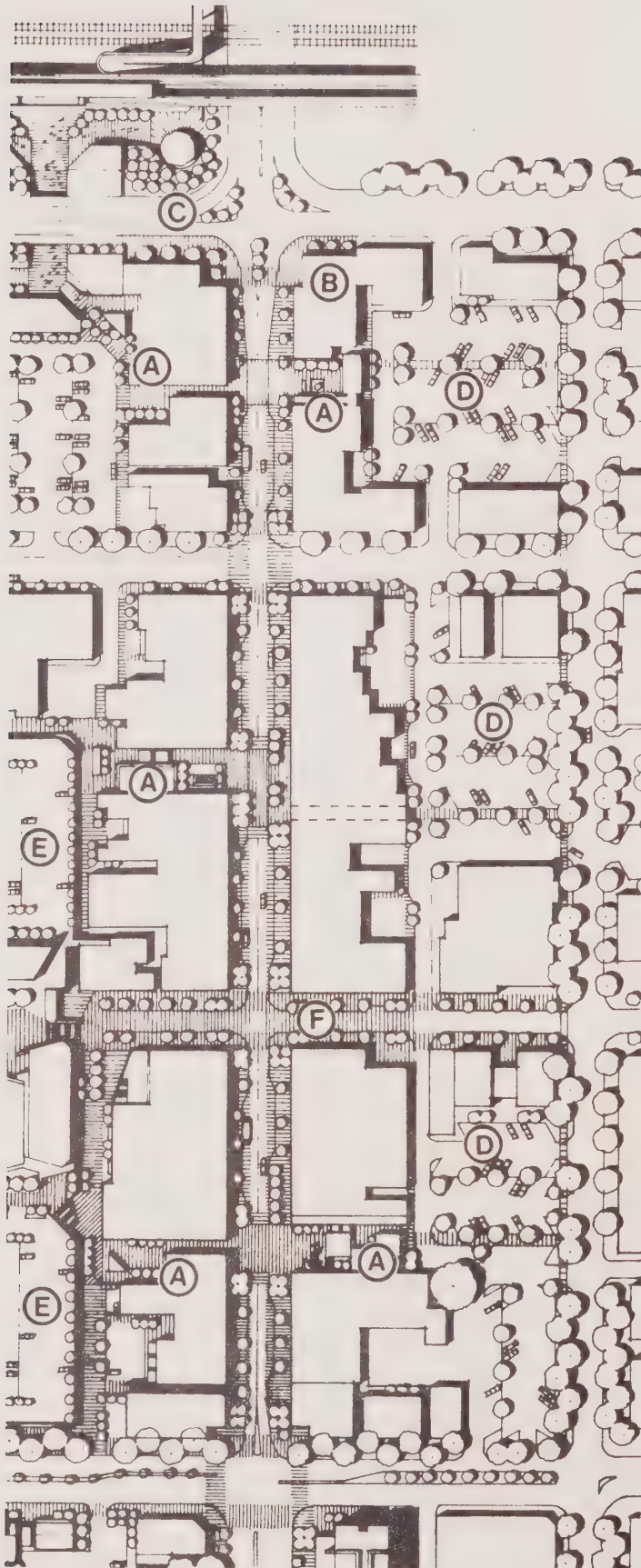
Street Dimensions. A roadway width of 28 feet (accommodating one lane in each direction) should be provided for vehicular use. Bus loading and unloading zones should be provided as shown. A minimum sidewalk width of approximately 14 feet shall be maintained along the street with width increased to at least 22 feet at all end-block and mid-block crosswalks. This width should be maintained for a minimum distance of 25 feet on each side of a mid-block crosswalk and 25 feet on the interior side of end-block crossings. A limited number of short-term parking spaces and passenger dropoff bays may be provided where they do not interfere with either auto, bus or pedestrian use.

Right-of-way Surfaces. Paving materials along Castro Street should, by changes in color, texture and pattern, convey the differences in usage of various street spaces. The continuity of pedestrian use of sidewalks should be stressed at crosswalks by employing the same paving materials. This also serves to visually tie together pedestrian areas along each side of the roadway, and stresses the functional relationships that should exist along both building frontages. The travel lanes for autos should be distinguished from pedestrian spaces by changes in materials which demark the lanes, visually and physically, by slight differences in elevation and texture. Provisions should be made at end-block crossings to eliminate curbs by use of ramps and at mid-block crossings by maintaining crosswalks at the same elevation as adjoining sidewalks.

Paving materials for pedestrian areas should be precast concrete, brick or tile paving units. Tones of gray or slate should be used to convey an urban quality and to provide a surface which will age well. The same paving material should be used in the bus and parking bays to distinguish them from auto travel lanes. Within the roadway section asphaltic concrete or concrete colored to a dark gray tone should be used.

A level surface should be maintained along the street to achieve an uncluttered appearance and allow flexibility in the use of the street space. However, at the California Street and Evelyn Avenue corners a raised section or wall one-and-one-half to two-and-one-half feet in height could be provided to accentuate the

**Figure 10**  
**ILLUSTRATIVE PLAN**  
**CASTRO STREET**



- (A)** Mini-Plazas
- (B)** Land Assemblage - Proposed
- (C)** New Road Alignment - Later Phase
- (D)** Reorganized Offstreet Parking Lots
- (E)** Parking Structures - Later Phase
- (F)** Redesign of Dana Street





Note: Sign Control Except where Attached to Canopies. Eliminate Projecting Commercial Signs. Restrict Placement of Business Identification Signs to a Maximum of 14 Feet



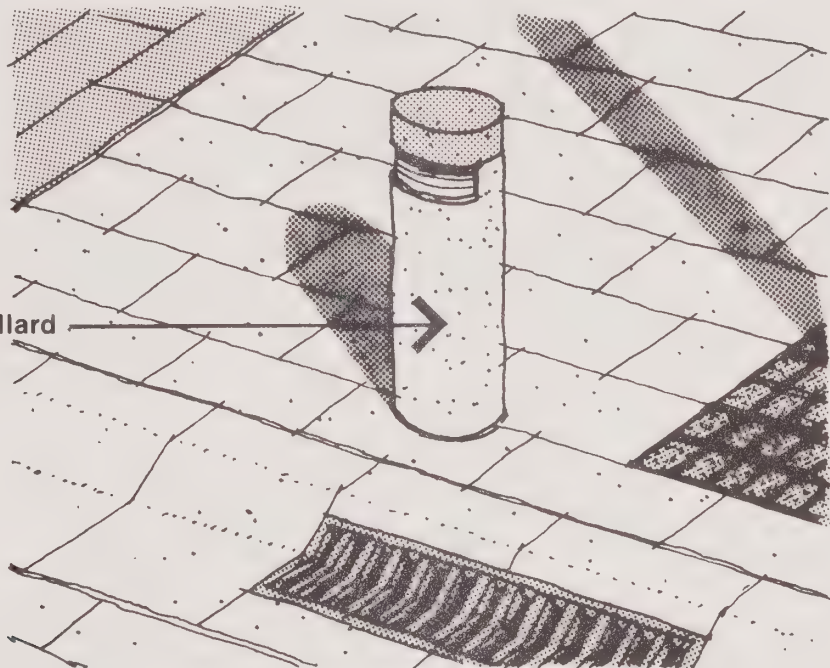
Figure 11  
ILLUSTRATIVE  
CASTRO STREET ELEVATIONS



entrances to the main retail area of Castro Square. No curb cuts should be permitted on Castro Street for vehicular access to private properties.

**Figure 12**  
**RECOMMENDED**  
**PAVEMENT DESIGN—**  
**CASTRO STREET**

Light Combined With Bollard



Plant Materials. Landscape materials on Castro Street should be limited to street trees supplemented by annual flowers in movable concrete planters. Except at the intersections at California Street and Evelyn Avenue, a single variety of large, round-headed deciduous trees should be used along Castro Street to provide shade in the summer and permit sunlight during the winter months. This tree should be of a type to form a significant canopy which will help to better define the street space. Either the London Plane Tree (*Platanus acerifolia*) or Tulip Tree (*Liriodendron tulipifera*) should be used. At the California Street and Evelyn Avenue intersections, a broadleaf evergreen species such as Evergreen Ash (*Fraxinus uhdei*) or Evergreen Elm (*Ulmus parvifolia*), should be used to define the entrance into the main pedestrian area of Castro Square.

Lighting. In order to minimize the undesirable visual effects caused by too many vertical elements, the use of 42-inch high bollards incorporating integral lighting is recommended along the street, spaced approximately 10 feet apart. Supplementary lighting should be provided at crosswalk areas by use of luminaires with globe



clusters. A maximum mounting height of 14 feet will help to maintain a scale in keeping with the pedestrian character of the space. Other supplementary lighting should be provided by tree mounted lights and flush ground lights.

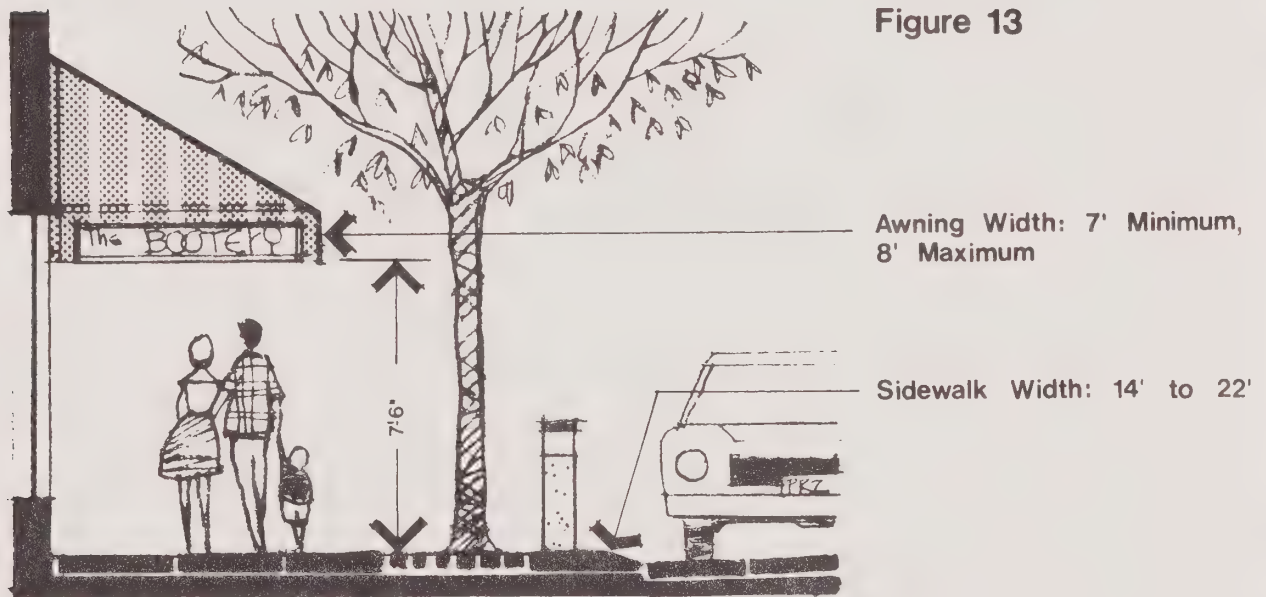
Commercial Signs. The commercial sign requirements are designed to promote the following aims: 1) ensure that all commercial establishments are provided with adequate means of identification; 2) achieve a pattern of signing which is primarily intended for viewing by pedestrians and easily seen from pedestrian-oriented areas; 3) provide signs which are in scale with and do not dominate the architecture of the building to which they are attached; 4) eliminate random placement of signs (which often obscure each other and thus confuse rather than inform); 5) promote a sense of visual interest and vitality to the area through use of well-designed, colorful signs; 6) coordinate signs with landscaping and street lighting; and 7) assist in promoting a system of coordinated awnings and canopies along the street frontage as a means of unifying and enlivening the appearance of the street. The following provisions shall apply.

1. One identification sign per building shall be permitted along Castro Street, Dana Street, Villa Street and California Street. The sign shall not exceed two percent of the surface area of the building facade to which it is related and in no case shall it exceed 60 square feet. Such signs shall be attached to the building and designed as an integral part of the architecture of the structure. They shall not project above the eave or parapet of the building and no part of the sign shall project more than 18 inches from the building face. If illumination is provided it shall be indirect; i.e. from a concealed source, not from within the sign.

2. Signs identifying building occupants shall be permitted along Castro Street, Dana Street, Villa Street and California Street, provided that the total sign surface per building face does not exceed 15 percent of the surface area of the related building face for single story buildings or 20 percent of the surface area of the first story portion of multi-story buildings. The placement of such signs shall be limited to the first story of the building face or a maximum height of 14 feet above the ground surface. The signs shall be applied or attached to the building face and shall not project more than 18 inches from the building face. Illumination shall be indirect (from a concealed source) or internally illuminated.

3. In addition to identification signs permitted above, one two-faced sign with a set dimension of one foot by six feet shall be permitted, to identify ground level occupants occupying street frontage space or space directly accessible from the street. Such signs shall be located no closer than 20 feet apart and shall be sus-

pended from an awning or canopy. A uniform height of seven feet, six inches, above the ground surface shall be maintained for all such signs. Signs shall be either indirectly illuminated or internally illuminated.



4. In addition to signs permitted in provisions 1, 2 and 3 above, signing as an integral part of an awning or canopy may be permitted, conditioned upon design review.

5. A 20 percent increase in the sign area of attached identification signs shall be permitted on the secondary frontages of Castro Street buildings which face onto public off-street parking areas if done in conjunction with a remodeling of the rear building face.

6. A 10 percent increase in the allowable surface area of identification signs permitted in provisions 1, 2, and 5, shall be conditionally permitted where such signs are judged to provide a positive contribution to the appearance of the area. Any or all of the following criteria may be employed in making that determination:

1) the sign represents a superior example of contemporary graphic arts; 2) colors, materials or textures used in the sign help enliven the appearance of the street

space; and 3) the sign is an integral part of the architectural design of the building and enhances the building design.

Building Heights. A maximum building height of 40 feet and a minimum building height of 25 feet shall be required for all new buildings, with the exception of a portion of the block bounded by Castro Street, Evelyn Avenue, Bryant and Villa Streets. In this area heights up to 80 feet are permitted as shown in Figure 9.

Building Siting. No setbacks or sideyards shall be permitted for buildings along Castro Street, in order to: 1) provide a continuous and uninterrupted commercial frontage; 2) create and maintain a well-defined street space and; 3) promote greater site utilization. These requirements shall also apply to Dana, Villa and California Streets for a distance of approximately 100 feet from Castro Street.

Building Design. Building design shall conform with previously specified major urban design policies 6 and 7.

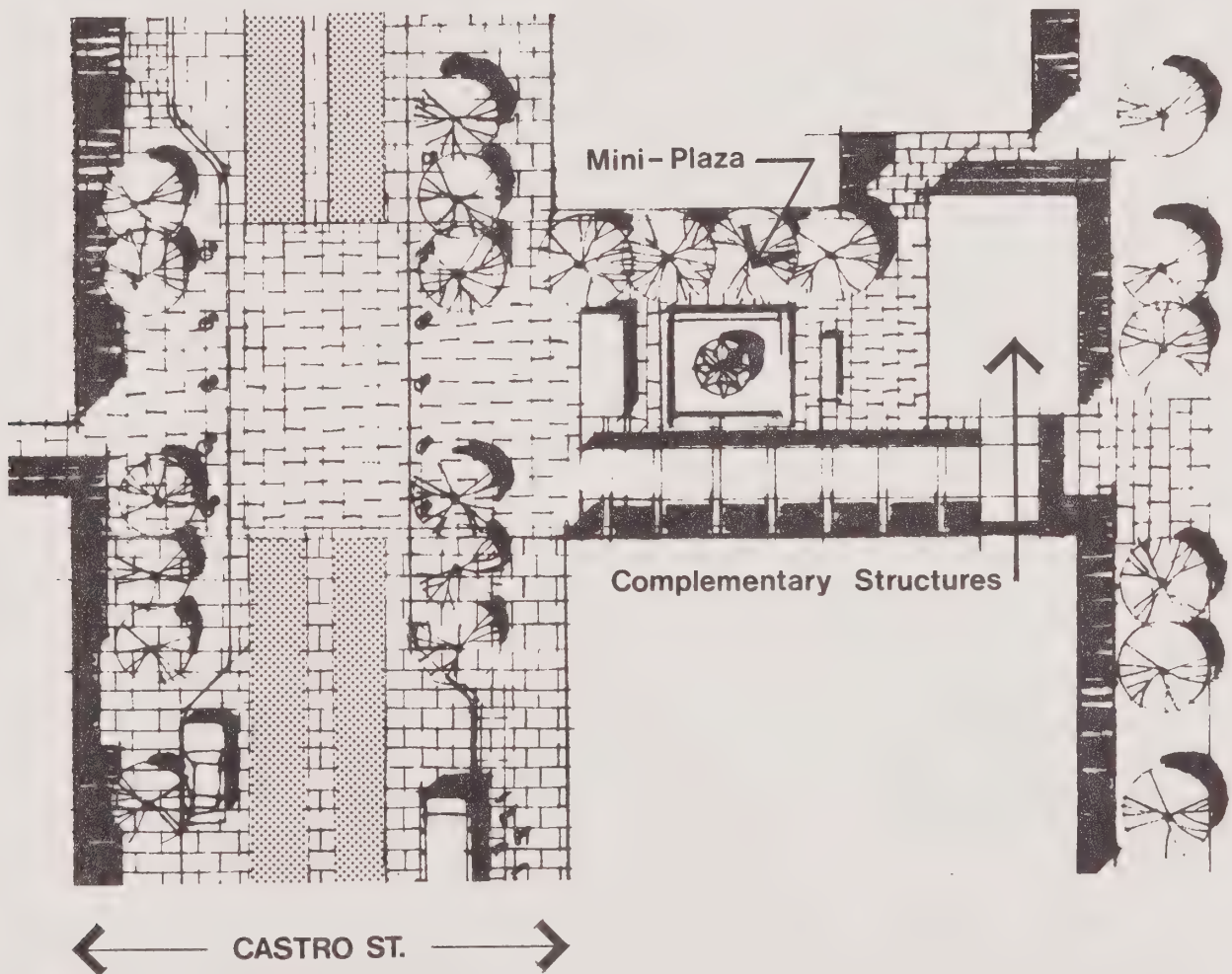
**MINI-PLAZAS.** Five small plazas are proposed which are to link Castro Street with peripheral parking facilities. The purpose of these plazas is to provide more pleasant and direct pedestrian connections between Castro Street and the major parking areas, to provide suitable locations for pedestrian oriented activities, and to create opportunities for relating commercial activities to public areas. Three of these plazas open directly onto Castro Street and require acquisition of existing buildings.

In conjunction with the development of the mini-plaza on the east side of Castro Street, between Villa Street and Evelyn Avenue, three adjoining privately owned parcels to the north should be acquired. It is proposed that these parcels, one of which is vacant, a second occupied by a card room and the third by the Fraternal Order of Eagles, be cleared and combined with the City owned parcel at the corner of Castro Street and Evelyn Avenue. Concurrently with development of the mini-plaza this enlarged parcel should be marketed for development purposes as specified in the Plan. Figure 14 illustrates development of the mini-plaza.

Property acquisition costs would be partially or totally offset by sale or lease of a portion of the acquired site for related retail uses. Building frontages facing onto these plazas are classified as primary commercial frontages and subject to the same use restrictions in effect along the balance of Castro Street. Special effort should be made to bring in retail uses which will both benefit from the plaza siting and contribute to its vitality. Building sites created through mini-plaza development



would also be appropriate for public uses, such as bus shelters, child-care facilities and public restrooms.



**Figure 14**  
**TYPICAL MINI-PLAZA AND PAVEMENT DESIGN ALONG**  
**CASTRO STREET**

Two additional plazas are provided along the secondary frontage and use already vacant land. Additionally, improvements should be made to the passageway through the Minkler Building to upgrade the appearance of this connection. The following design conditions shall be observed in the design of these spaces.

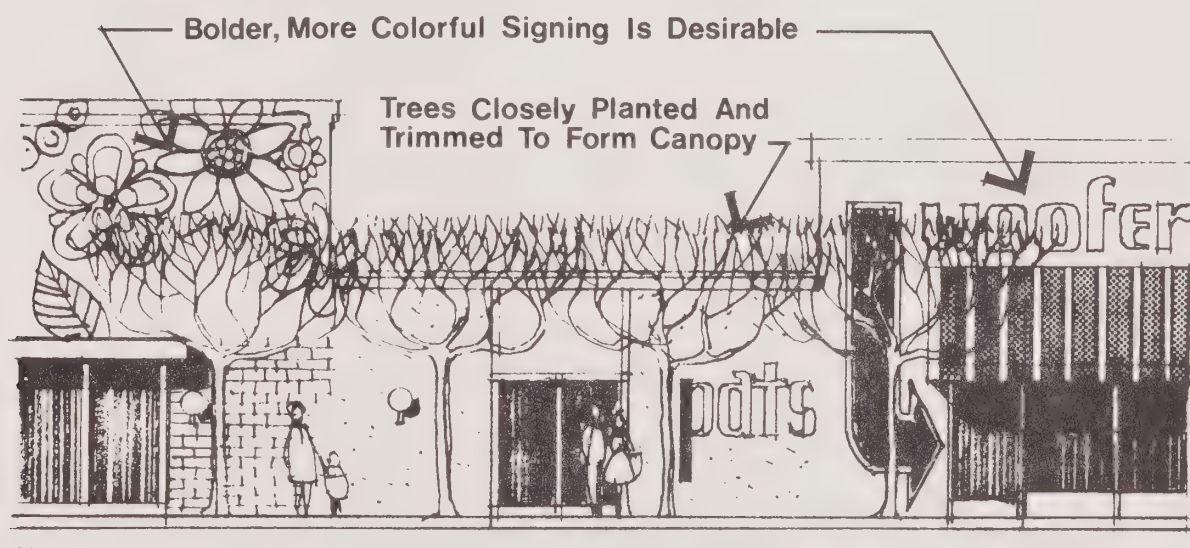
1. Plant materials used in these spaces should provide a distinctive identification for each. A degree of informality in the landscaping is appropriate as a contrast with the more formal landscape treatment of Castro Street.
2. The paving materials used should be, for the most part, the same as those employed on Castro Street in order to emphasize these spaces as links between parking areas and Castro Street. Here, however, the patterns could be changed to call attention to the difference in function and some variation in paving materials, or the use of plant groundcovers, would be appropriate.
3. Illumination should be provided by wall-mounted lighting, flush-ground lights, tree-mounted lights, wall surface flood lighting from concealed sources, or some combinations of these methods. Pole-mounted lights should be avoided in order to minimize visual disruption of the space except where they serve as a means of deliberately defining areas within plazas or delineating circulation patterns.
4. A free-standing or wall-mounted directory sign identifying both ground level and upper level occupants should be provided for each block along Castro Street. These signs should be provided at both the parking lot and Castro Street entrances to the plazas.

REAR FACADES. Since a majority of Castro Square visitors approaching by automobile will receive their initial impressions of the area from the public parking facilities and rear facades of shops facing onto these parking lots, significant attention should be given to improving the visual quality and impact of these secondary frontages. The following steps are proposed.

1. Phase out scattered private parking spaces which functionally and visually disrupt the area. A five-year removal program, timed to coincide with scheduled additions of public parking spaces, is proposed. Exceptions could be made for present lessees with leases extending beyond five years if use of parking spaces is included in the lease terms. In such cases the restriction of private parking would go into effect upon expiration of the present lease. Similarly, parking spaces used by a business on the site operated by the property owner may be exempted. However, if the present building is replaced, the prohibition on on-site parking shall apply.
2. Consolidate trash storage areas and conceal from public view.

3. Provide a unified paving surface along the rear facades of buildings, including both sidewalks and areas used for private parking or service access, to help eliminate the present chaotic appearance.

4. Small scale trees should be planted and pruned to form a canopy along the edge of parking areas adjoining the backs of buildings, providing a unifying element for the diverse facades. A different type of tree should be selected for each block to reinforce local identification. The following trees are recommended for use in these locations: Washington Thorn (Crataegus phaenopyrum), Crabapple (Malus floribunda), Myoporum (Myoporum laetum), and Evergreen Pear (Pyrus kawakamii) and Loquat (Eriobotrya japonica).



**Figure 15**  
**LANDSCAPING OF REAR FACADES**

5. Illumination should be provided by means of tree-mounted lights.

6. Commercial sign provisions specified for Castro Street frontage shall apply here, except that a 20 percent increase in the allowable size of an identification sign attached to a building face shall be permitted if designed and incorporated with a remodeling of the building face. Bold, colorful signs painted directly on the wall surface are considered desirable.



7. Encourage private remodelings of rear building facades facing onto parking lots to upgrade the general appearance, but discourage provision of major public entrances to Castro Street businesses from parking areas. Instead, try to direct visitors through the entrance mini-plazas onto Castro Street and in this way maximize pedestrian exposure to Castro Street goods and services.

**OFFSTREET PARKING AREAS.** Offstreet parking areas occupy a major portion of the area's land supply and are the major points of entry to Castro Square. Thus they constitute a major design element. The following modifications should be undertaken. The accompanying diagram illustrates a typical application of these provisions.

1. Parking areas should be reorganized where necessary to meet adopted city parking standards, as to width, depth and aisle space.

2. Ingress and egress to parking areas should be reduced to eliminate traffic conflicts with the adjoining streets. Upon entering the parking lots all parking spaces should be accessible without reentering the street. (At present most parking lots are laid out in a manner which uses the street as a part of the lot's internal circulation system.)

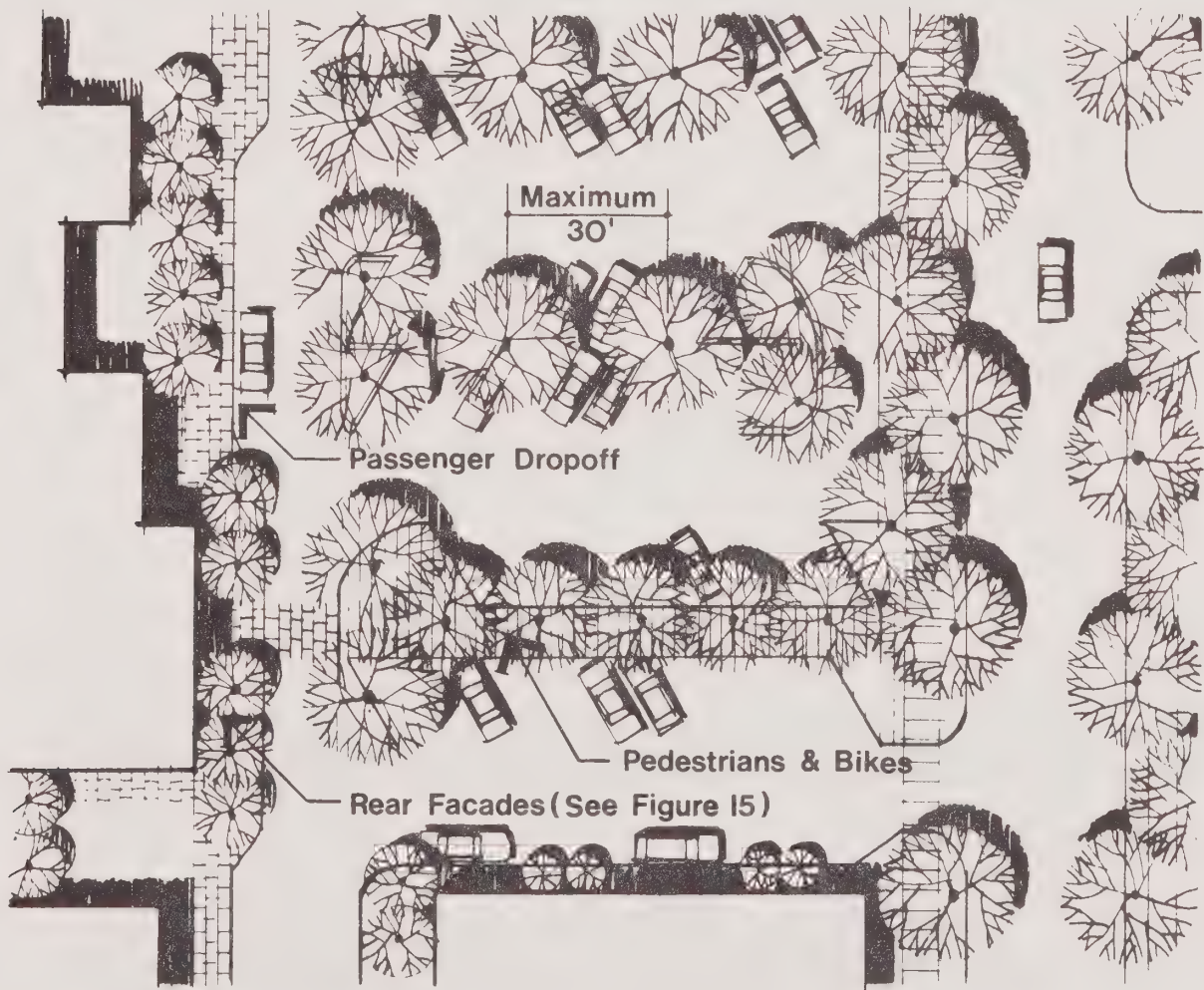
3. Existing lighting fixtures should be removed and replaced by lighting fixtures which provide a more uniform level of illumination and which are more in scale with the area. Tree-mounted lights are recommended. In early stages of growth, the lights could be attached to supports provided for the young trees.

4. Lots should be landscaped with street trees which provide shade in the summer and protection from rain in the winter season. Therefore, round-headed broad-leaf evergreen or coniferous trees, such as Southern Magnolia (*Magnolia grandiflora*) or Italian Stone Pine (*Pinus pinea*), are recommended. The trees should be spaced to provide a canopy over the parked cars and main pedestrian access routes. The recommended spacing of the trees is illustrated in Figure 16.

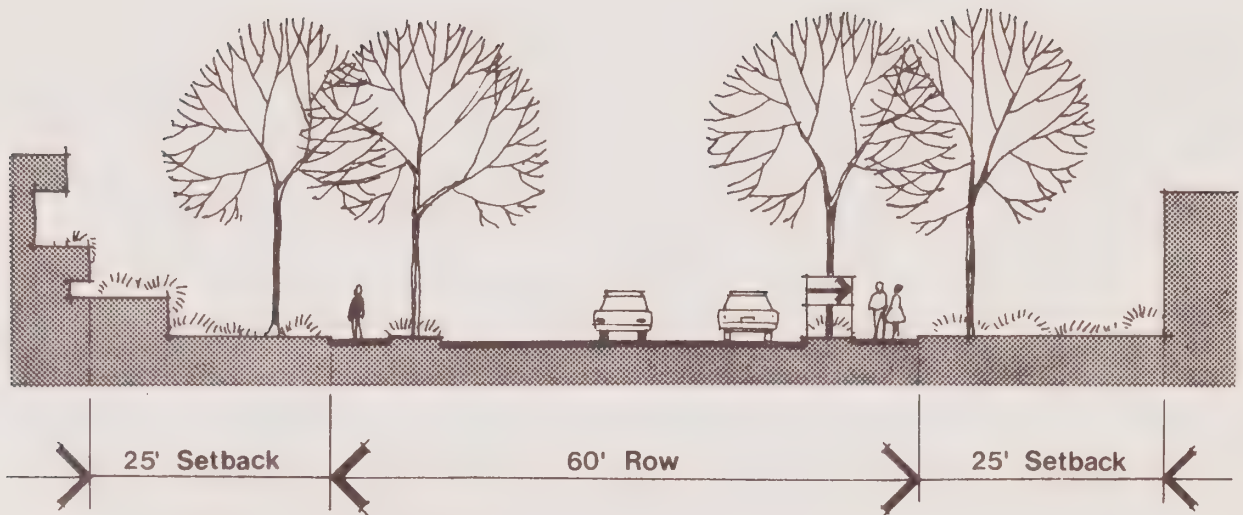
**PERIMETER ROADS.** The following roads form parts of the distribution loop road system: Franklin Street, Villa Street, California Street and Hope Street. The design conditions listed below apply to these roadways.

1. A single variety of trees shall be planted along these streets to help visually

define their function and interrelationships. Trees should be planted in a formal manner, i.e. with uniform spacing. However, at intersections with related perimeter roads a closer spacing should be used for emphasis. At entrances to major facilities served by the roadway a wider tree spacing should be used. The tree type selected should have a round or fan-shaped head, with moderate or fast growing characteristics, and should grow to a height which permits clearance for trucks while spreading out over the street space to create a tunnel or canopy effect. Southern Magnolia or Italian Stones Pines suggested for parkings areas are also recommended here.

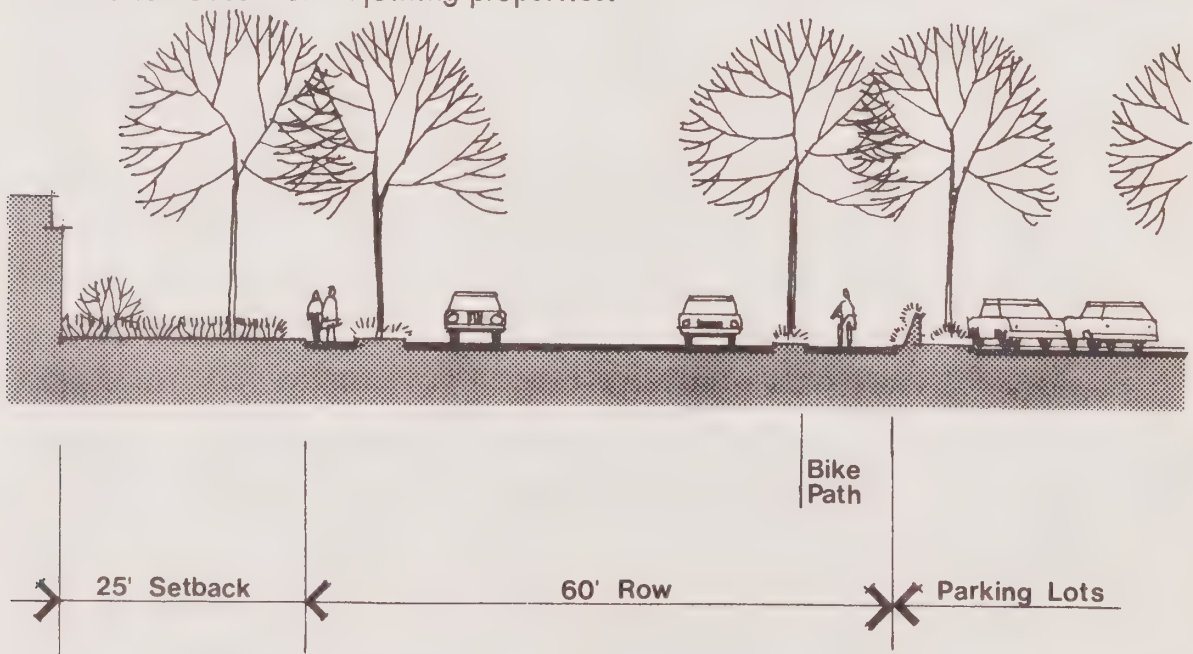


**Figure 16**  
**PROPOSED REORGANIZATION AND LANDSCAPING**  
**OF OFFSTREET PARKING LOTS**



**Figure 17 FRANKLIN STREET**

2. Along Franklin Street the street trees should be supplemented by a parallel planting of trees of the same type within the required frontyards of adjoining properties. Where possible these trees should be planted in conjunction with street landscaping. In some instances, however, planting will only be possible when new construction occurs on adjoining properties.



**Figure 18 HOPE STREET**

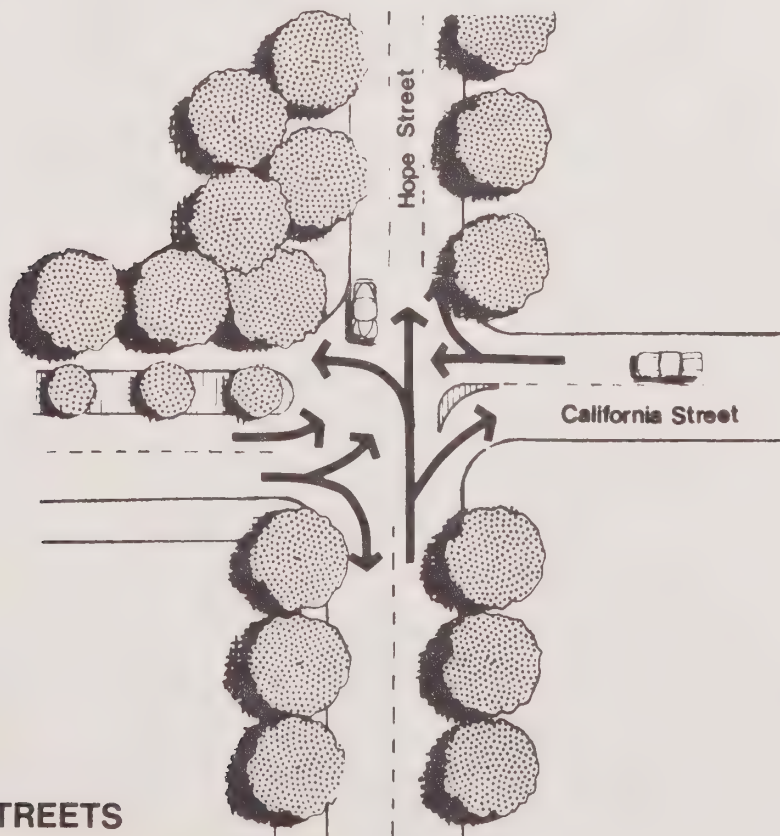
3. Hope Street should have a parallel planting of trees equivalent to that specified



on Franklin Street. A low wall, approximately three feet in height, should separate parking areas from sidewalks within the road right-of-way. Vines should be planted along the base of the wall to soften its appearance.

4. With the exception of Villa Street, between Franklin and Hope Streets, the roadways shall be defined by a continuous five to six feet wide planting strip along the curbside. The plant material used should be limited to a single type except at street intersections and entrances to major parking facilities. At these locations mass plantings of annuals are recommended to lend color to the street scene and emphasize these functional interruptions along the street.

5. The design requirements for the California-Hope Street intersection are shown in the accompanying diagram. Here the intent is to clarify the connection between these two roadways and to stress the continuation of auto movement from California to Hope Street.



**Figure 19**  
**CALIFORNIA – HOPE STREETS**

6. All overhead utility lines shall be removed along these routes.

7. Street lighting shall be provided by pole-mounted lights not to exceed a height of 18 feet. This places the lights at a height approximately even with the lower branches of street trees as they mature, eliminating the need to severely trim tree

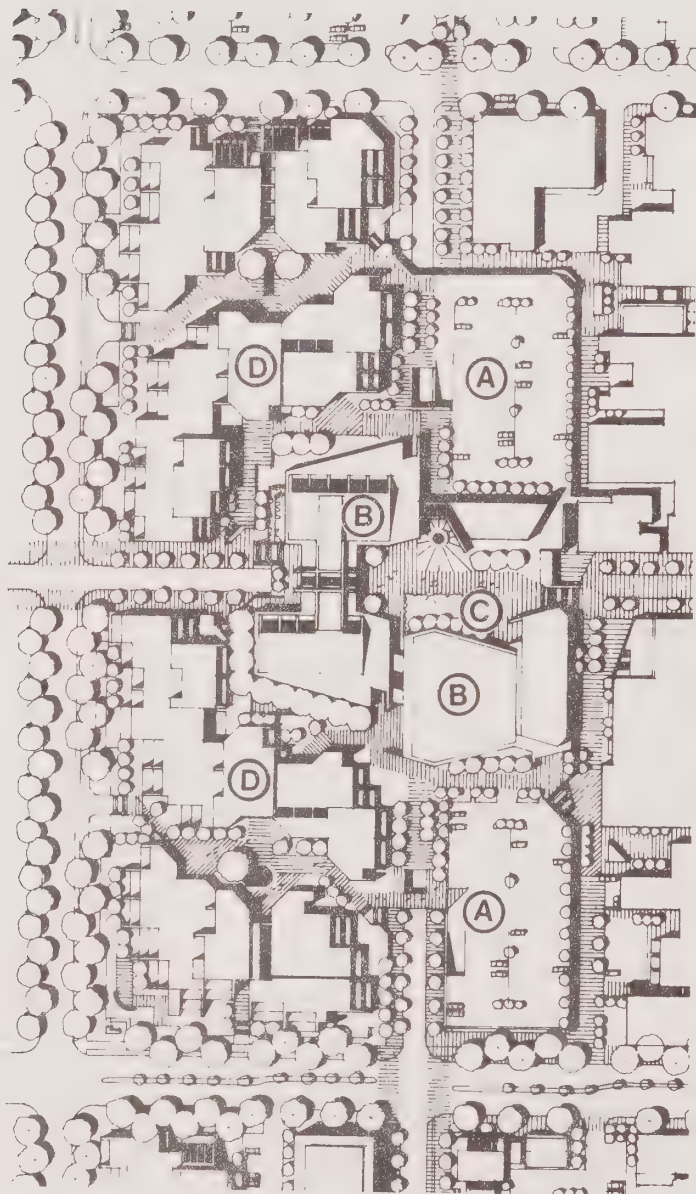


Figure 20  
ILLUSTRATIVE PLAN  
DANA PLAZA

- (A) Multi-Level Parking
- (B) Cultural Facilities
- (C) Public Plaza
- (D) Multiple Housing above Parking

branches back. Moreover, it maintains a scale in keeping with the adjoining buildings. Poles shall be placed in line with street trees so as to reduce their visibility.

8. Access to offstreet parking shall be consolidated to minimize conflicts with vehicular movements on perimeter streets. A maximum of three driveway cuts per block face shall be allowed along Franklin and Hope Streets. Within the Oak Street residential area, vehicular access to offstreet parking should be provided from Oak Street where possible.

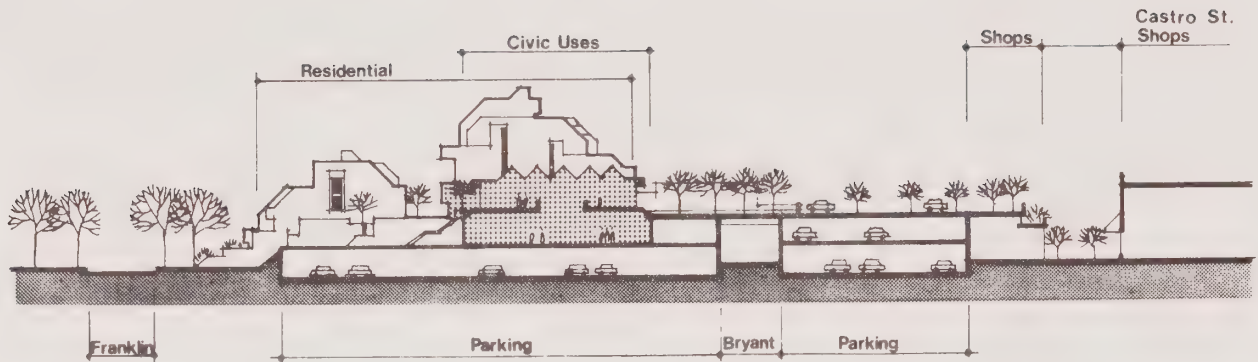
9. Directional signs identifying major offstreet parking facilities should be provided along each of these streets.

DANA STREET. During initial stages of the program the portion of Dana Street from Hope to Bryant Street should be reconstructed to provide for diagonal parking. The same landscape treatment employed along Castro Street should be used here as well. In a later phase of the program, following expansion of offstreet parking, provision of neighboring civic facilities, and greater intensification of pedestrian activity, diagonal parking should be phased out and the area converted to pedestrian use. At that time the roadway surface should be repaved with the same materials used on the sidewalk areas. Ingress or egress to offstreet parking should be permitted provided that parking accessways are clearly delineated by bollards.

DANA PLAZA. Figures 20 and 21 provide an illustrative plan and diagrammatic section through the Dana Plaza area and depict the basic design concept. Civic uses would be organized about a central plaza link to Castro Street by way of Dana Street. The plaza would be raised to permit continuation of vehicular movements along the existing Bryant Street right-of-way. The intent, however, would be to facilitate circulation between the adjoining parking facilities and not to provide for through traffic movements. Multi-level parking would be provided on the sites now devoted to off-street parking, and adjoining lands along the west side of Bryant Street recommended for acquisition for parking in the initial phases of this program. The balance of the Dana Plaza area would be devoted to residential use. The following design and development conditions should be observed.

1. All of the properties along the east face of Bryant Street, from California Street north to the City owned parcel, used as access to the police and fire facilities,





**Figure 21**  
**DANA PLAZA - ILLUSTRATIVE SECTION**

plus the remaining private property on the southeast corner of Bryant and Dana Streets, should be publicly acquired and put to use for offstreet parking. At such time that civic facilities or housing are ready for development the parking should be reorganized and multi-level parking provided on all or a portion of the parking sites.

2. Parking structures facing onto the rear of Castro Street businesses should incorporate commercial spaces at ground level to provide pedestrian interest.
3. The Dana Plaza development should provide for convenient pedestrian access from the Oak Street neighborhood to Castro Street via Dana Street.
4. Lands designated for residential use should be publicly acquired and assembled into more useable parcel sizes. These properties should then be sold or leased to a housing corporation, either non-profit or for-profit, which can guarantee a mix of housing consistent with the policies of this Plan and the City's housing element.
5. Building heights in the residential portion shall be limited to a maximum of eight stories.
6. At least 60 percent of all dwelling units shall have direct access to outdoor living space. Where such space is provided solely for use by the adjoining living units it shall have a minimum area of 100 square feet and a minimum dimension in any direction of seven feet.
7. Side and rear yards shall not be required. Proper siting of structures shall be

determined by regulations fixing criteria for location of windows (to insure natural light for and views from major living spaces) and the relationship of windows in adjoining structures, whether on the same or on an adjacent lot. Major living spaces are defined as any of the following: living rooms, dining areas, bedrooms or other spaces intended to serve any of these purposes. The siting of buildings shall be governed as follows. (a) A building may be sited on the property line provided that it has no windows facing the property line and that there is no building on the adjoining property within 15 feet which has windows serving major living spaces. In no case, however, can the face of the building sited on the property line exceed a height of 20 feet above the pre-existing ground elevation. (b) The face of any residential building, the first two stories of which contain windows serving major living spaces, shall maintain a minimum distance of 15 feet from any other residential building face without windows serving major living spaces, and a minimum distance of 20 feet from one containing such windows. The space between such buildings shall be increased by 10 feet for each additional story above two.

8. Any portion of the site used for parking which is decked over and constructed for open space use, serving either individual residential units or the entire project, shall be excluded for purposes of calculating allowable lot coverage.

9. Building materials and colors shall conform with the major urban design policies of this plan.

10. Along Franklin Street frontage a fence up to six feet in height may be provided if set back at least two feet from the property line.

11. Vehicular ingress and egress shall be limited to Bryant Street and Dana Street, where there shall be common access for parking serving public, commercial and residential uses, and to Franklin Street. In the latter instance no more than two curb cuts shall be allowed per block. To achieve this it may be necessary to coordinate and consolidate parking facilities serving the residential portion of the Dana Plaza development.

**EAST CORRIDOR.** The intent of the following conditions is to create a compatible relationship between the residential neighborhood to the east and the more intensive commercial use of Castro Square to the west.

1. No building or structure shall exceed three stories or 35 feet in height. Along the View Street frontage building heights at the frontyard setback line shall be limited to two stories or 24 feet. Heights in excess of this shall be permitted if set back an additional 25 feet from the required frontyard setback line.

2. Along the View Street frontage the street landscaping should be less formal than that used along the perimeter road network. Street trees selected should provide a canopy over the street space. Street lighting should be provided by pole-mounted globe lights not to exceed 14 feet in height.

3. Sign provisions of the (O) District shall apply except within residentially designated portions, where they shall be modified to permit only signs attached to buildings.

4. View Street should be used by vehicles primarily for residential access. The main vehicular access to all commercial uses should be from Hope Street.

5. Dana Street should serve as the main pedestrian and bicycle link between Castro Square and residential neighborhoods to the east. The pedestrian function of Dana Street should be reinforced by provision of an internal pedestrian walkway system linking development on the adjoining blocks with Dana Street, as shown in Figure 20.

6. All building placement, open space and ground coverage conditions specified in the Dana Plaza subarea discussion shall apply (i.e. points 6,7 and 8.)

**CIVIC OFFICE CENTER.** Major urban design provisions for this area are shown in Figure 22. The applicable provisions are as follows:

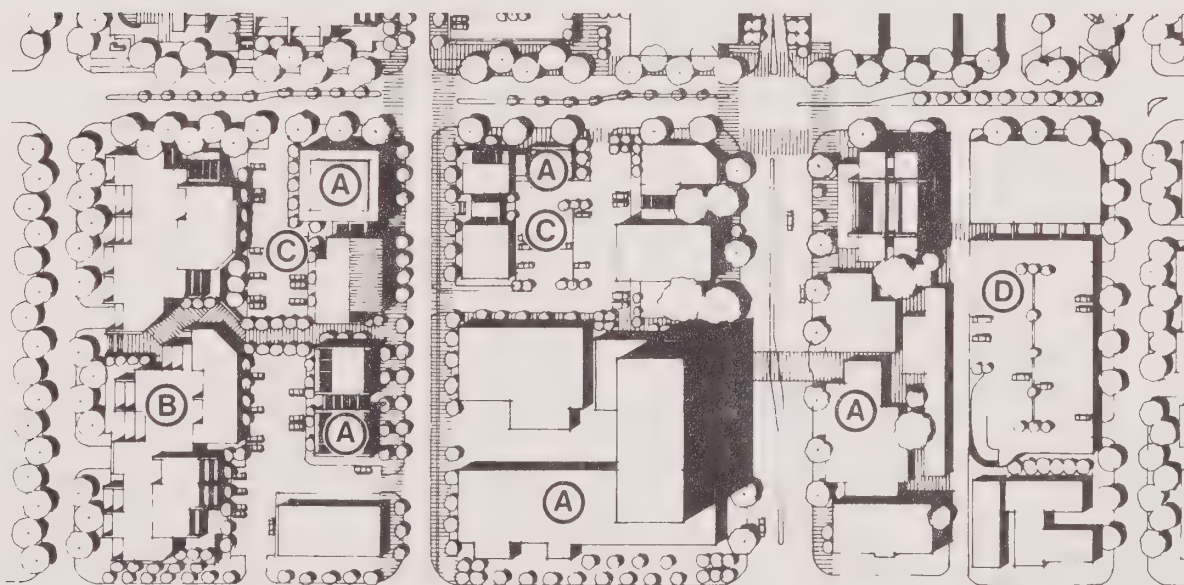
1. Buildings shall be sited along Castro Street to maintain a continuous, uninterrupted building facade. Sideyards shall be prohibited except where they serve as access to parking or as pedestrian passageways to uses within the center of the block. Frontyard setbacks on the west side of Castro Street shall maintain the setback line established by the IED Building. On the east side of Castro Street a 20 to 25 foot setback shall be required on the parcel on the southeast corner of Castro and California Streets, while on the northeast corner of Castro and Mercy Streets a setback of up to 25 feet is optional.

2. Provisions similar to those outlined for Castro Street shall apply along Bryant



Street to reinforce its role as a major pedestrian link between existing Civic Center facilities, planned residential, cultural and commercial uses in the Dana Plaza area, and the regional transit facilities immediately to the north of Dana Plaza. Minor variations in building setbacks along Bryant Street shall be permitted provided that the setback does not exceed 25 feet and the space provided by the setback is designed as an integral part of the adjoining pedestrian area.

3. Required buildings heights are shown in Figure 9. However, along Castro and Bryant Streets more exacting height controls shall apply. All portions of buildings within 30 feet of either of these two street frontages shall maintain a minimum height of 24 feet and a maximum height of 40 feet.



**Figure 22**  
**ILLUSTRATIVE PLAN CIVIC OFFICE CENTER**

- |                              |   |
|------------------------------|---|
| (A) Office-Professional Uses | (C) Possible Sites for Public Administrative Facilities |
| (B) Multiple Housing         | (D) Multi-Level Parking if Needed                       |

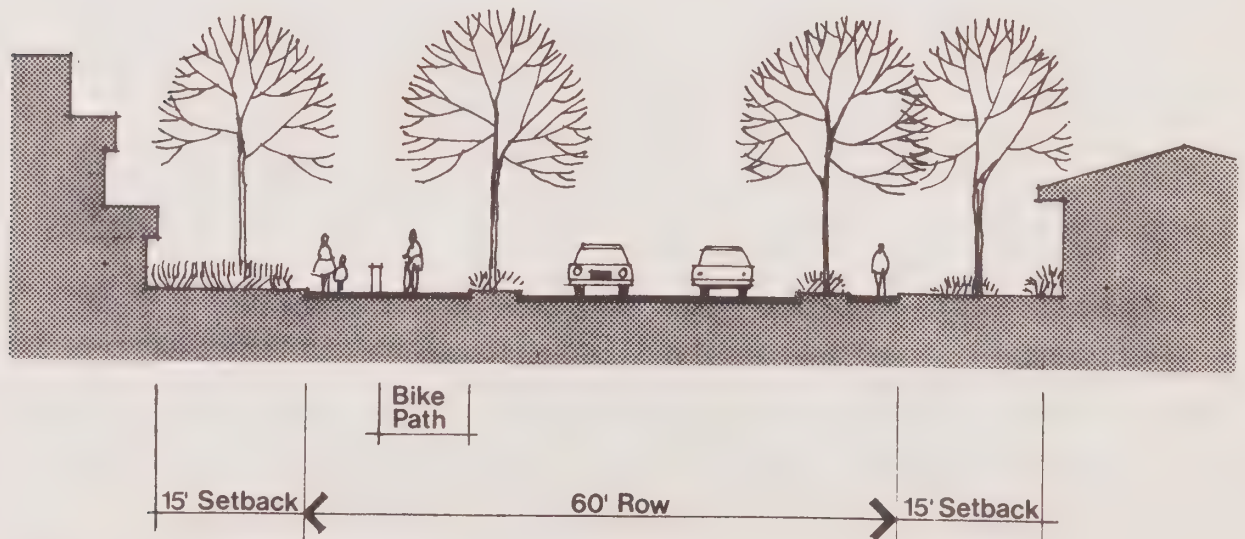
4. Bryant Street between California and Mercy Streets and Mercy Street between Franklin and Castro Streets are designated as primary pedestrian areas. In these areas, through vehicular movements shall be restricted and limited to access to parking provided in conjunction with adjoining uses. A uniform paving surface could be used for both pedestrian and auto spaces, with bollards used to physically separate these functions. Street landscaping on Bryant Street should stress the linear connection with Dana Plaza and the Civic Center. Street lights should be pole-mounted globe lights not exceeding a height of 14 feet.

5. Offstreet parking shall be sited in the interior portions of blocks and concealed from Castro and Bryant Streets.

6. Residential development shall conform to all residential provisions set forth in the Dana Plaza Subarea requirements.

**OAK STREET AREA.** Within this neighborhood the residential requirements set forth in the Dana Plaza subarea provisions shall apply. These are supplemented by the following conditions.

1. Dana Street should be closed between Oak Street and Bailey Avenue and the abandoned right-of-way incorporated as a part of the adjoining park.



**Figure 23 DANA STREET**

2. Between Oak and Franklin Streets the Dana Street right-of-way should be constructed as shown in the accompanying sketch. This same street design should be carried through to Dana Plaza. No additional curb cuts should be permitted along this street and existing curb cuts should be eliminated as existing structures are removed and new facilities provided. Either bollards or a low wall, not exceeding two to three feet in height, should be provided along the north side of the street to separate pedestrians and bicyclists. Frontyard setbacks along this frontage may be reduced to 15 feet. Street landscaping should emphasize the linear connection to Dana Plaza and Castro Square, supplemented by parallel planting of the same type within frontyards of adjoining properties.

3. Residential uses should be organized about small central plazas or parklets with direct pedestrianway connections to Dana Street. Buildings should be sited and designed to channel pedestrian movements along these routes.

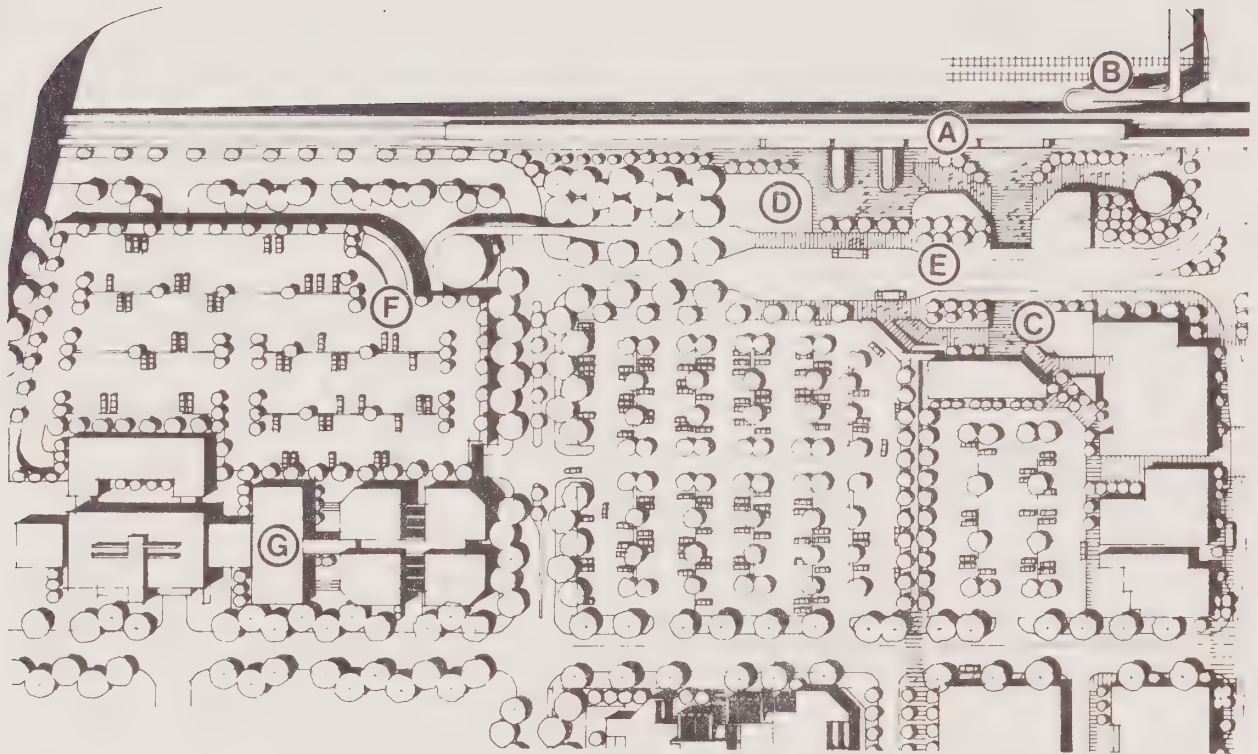
4. Along Oak Street, landscaping provided within the street spaces should avoid a formal, uniform appearance, using instead random planting and a greater variety of trees in order to contrast with nearby perimeter roads and Dana Street.

5. Vehicular access to residential uses should be concentrated along Oak Street to avoid conflicts with both the pedestrian function of Dana Street and the through-traffic functions of Villa, California and Franklin Streets.

TRANSIT STATION AREA. Primary urban design objectives for this area are: 1) provide an adequate site for a regional transit station; 2) create the design framework necessary to ensure that the subarea can become a major entrance to the city; 3) provide convenient and safe access to the transit station by foot, bus and car for people in all parts of the community; and 4) enhance access by transit to commercial and cultural uses in the Castro Square area. The following provisions are set forth to help promote these objectives.

1. Regional transit station facilities, which are approximately 700 feet in length, should be located between Castro and Franklin Streets. This area avoids conflicts with the proposed ramp connection between Bailey Avenue and the Central Expressway and provides more convenient access to the major activities of Castro Square. The station should be elevated to provide for continuation of Castro Street between Evelyn Avenue and the Central Expressway. A diagrammatic cross section through the station area is provided in Figure 25.

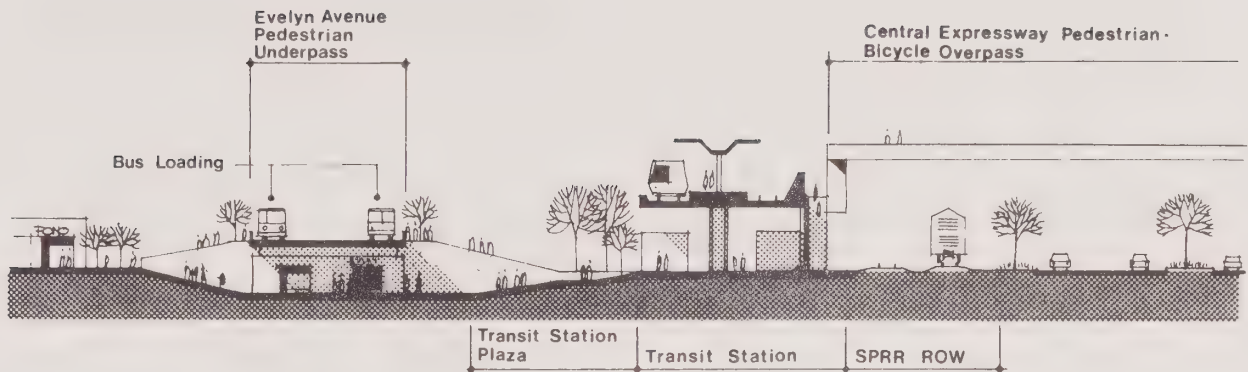




**Figure 24**  
**ILLUSTRATIVE PLAN TRANSIT STATION AREA**

- |                                  |  |
|----------------------------------|--|
| (A) Regional Transit Station     | (E) Bus Lanes and Boarding Areas                                     |
| (B) Pedestrian-Bicycle Overpass  | (F) Parking Structure with Connecting Pedestrian Overpass to Station |
| (C) Pedestrian-Bicycle Underpass | (G) Office-Professional Facilities                                   |
| (D) Plaza                        |  |

2. Multi-level, rather than open, on-grade parking should be provided for transit riders in order to reduce walking distances to the station, to minimize land acquisition for parking and thereby permit better land utilization, and to reduce adverse visual impacts created by extensive on-grade parking areas. Where possible, multiple use of parking sites for complementary commercial purposes should be encouraged. For instance, shops along ground level frontages or use of air rights for office development are recommended.



**Figure 25**  
**TRANSIT STATION AREA – DIAGRAMMATIC SECTION**

3. Grade-separated pedestrian access to the station from residential areas to the north should be provided by means of a pedestrian overpass across the Central Expressway and an underpass at Evelyn Avenue to permit direct pedestrian access from transit related parking and from Castro Square. Figure 25 illustrates these relationships.
4. Access by local and county buses should be accommodated by providing bus bays mid-block on Castro Street, between Villa Street and Evelyn Avenue, and via a realigned Evelyn Avenue. The latter would be one-way eastbound and partially elevated to permit construction of a ramped pedestrian underpass for the convenience of handicapped transit riders. Bus access to the station area could be routed from Bailey Avenue, via the proposed Evelyn Avenue loop beneath the Bailey overpass, or from Castro Street via Villa Street.
5. Protection from the weather should be provided for pedestrians approaching the station area from Bryant Street. This same area should serve as the discharge and waiting area for kiss and ride parking. Either dense plantings of broadleaf ever-green trees, a canopy, or a partially covered trellis would be appropriate.
6. Small commercial shops should be located in the area immediately adjoining the approaches to the pedestrian underpass and the commercial frontage extended to connect with the mini-plaza at the rear of the Castro Street properties.
7. A public plaza, partially sunken to accommodate the change in grade necessary for the pedestrian underpass, should be provided along the length of the station area.

8. Building heights should be limited to 40 feet with the exception of areas shown in Figure 9 permitting heights up to 80 feet.



### III. IMPLEMENTATION PROGRAM

The success of the Castro Square revitalization effort will depend upon a high degree of coordination between the application of regulatory provisions which govern development of privately owned land and the design, financing and construction of public improvements. These two primary implementing methods must be further complimented by techniques allowing for advanced acquisition of land, relocation of a limited number of families and businesses and technical assistance to property owners or businesses wishing to make improvements. Application of these methods are detailed below.

#### REGULATORY PROGRAM

The Plan for the Castro Square Area has been drafted to permit it to function as a "specific plan" as defined by Government Code Sections 65451-65452. Briefly, these government code provisions permit the planning commission to prepare, and the legislative body to adopt, "specific plans for the systematic execution of the general plan." Such plans may include proposed regulations limiting the use of land and buildings, heights, bulk, setbacks, provision of open space and the location, standards and use of all transportation facilities whether public or private. The plan may also include detailed regulations, conditions, programs, and proposed legislation which may be necessary or convenient for the systematic implementation of any general plan element.

In order to promote simple administrative procedures the Castro Square Plan incorporates, by reference, applicable provisions of the City's current zoning ordinance and supplements these with more specific requirements where essential for implementation of the Revitalization Plan. In some cases this results in application of more stringent requirements than those of the present ordinances. In most instances, however, the use of the specific plan technique permits greater flexibility in allocating uses and setting design requirements since provisions can be drafted to specifically represent conditions on the site. In contrast, since zoning provisions must be uniformly applied in various areas of the city, problems or opportunities unique to a given site cannot be considered.

Under this approach the Castro Square Revitalization Plan would be adopted by the City Council as a specific plan and concurrently existing zoning within the Project Area boundaries rescinded. It should be noted that the plan would also

supercede setback provisions established in Article IV of the City of Mountain View Zoning Ordinance. The specific plan would be administered by the city planning department staff in the same manner as they presently administer the City's zoning ordinance.

## **PUBLIC IMPROVEMENT PROGRAM**

Public improvements to be undertaken as a part of the Plan are summarized in Figure 26. The methods of financing these improvements are described below and are followed by a schedule of improvements.

**FINANCING METHODS.** Public improvement for Castro Square should be financed from six major sources:

1. Tax Increments. Tax increments produced by improvements in the project area since approval of the original revitalization plan now total approximately \$550,000. Based upon the present annual tax increments produced, an additional \$1,900,000 can be financed by use of 15-year bonds. Thus a total of about \$2,450,000 of improvements can be financed through tax increments. This excludes use of tax increments beyond the present level. It can be anticipated, however, that the tax increments will rise above the present level due both to inflationary trends which are likely to be reflected in land and property values and new development attracted to the area as a result of the revitalization effort. If the present rate of inflation continues it may be necessary to increase the tax increment funding beyond \$2,450,000. If this was to occur the portion of tax increment increase attributable to inflation should be used to finance these cost increases.
2. Improvement District. With the approval of the original revitalization plan an improvement district was established which is co-terminus with Parking District 2. This district was established with the understanding that approximately \$500,000 of improvements would be financed through the district. The schedule of improvements for Castro Square therefore includes these projected revenues. It is proposed, however, that the district boundaries be extended to include the entire area bounded by Franklin Street, Evelyn Avenue, View and California Streets.
3. Parking District No. 2. It is proposed that upgrading of present parking lots and improvement of public owned but unimproved lots be financed by the present



FIGURE 26

# SCHEDULED PUBLIC IMPROVEMENTS



- CASTRO STREET-RECONSTRUCTION AND LANDSCAPING
- PERIMETER ROADS-MODIFICATIONS AND LANDSCAPING
- RESIDENTIAL STREETS-LANDSCAPING
- DANA STREET-MODIFICATIONS AND LANDSCAPING
- BRYANT STREET-INTERIM LANDSCAPING
- MINI PLAZAS-ACQUISITION AND DEVELOPMENT
- REAR FACADES-LANDSCAPING
- OFF-STREET PARKING AND LOT NUMBER
- SITE AQUISITION AND DEVELOPMENT
- SITE UPGRADING

SEE TABLE 1 FOR PHASING OF IMPROVEMENTS

## CASTRO SQUARE REVITALIZATION PLAN MOUNTAIN VIEW, CALIFORNIA

SEDWAY/COOKE  
URBAN AND ENVIRONMENTAL  
PLANNERS AND DESIGNERS







parking district. Improvements required are estimated at about \$190,000 and thus require an annual assessment of \$24,000 to finance these improvements over the eight year program period.

4. Parking Deficiency District. At present parking costs generated by new uses are assessed against all properties in the District. This creates a number of inequities including the following: 1) no distinction is made between property owners who have been contributing toward retirement of the District's bonds for a number of years, and new investors who have access to the existing space but will be contributing only during the remaining and declining years of indebtedness and 2) improvement costs resulting from increased parking needs generated by new uses are assessed to all property owners, even those whose parking demand remains constant. To resolve these inequities an additional parking district, encompassing the entire revitalization area, should be formed. Within this district all new development producing parking demands in excess of those generated by present use of the site would be required to pay an in lieu parking fee sufficient to finance construction of parking as required in the City's zoning ordinance and provisions of this Plan. A parking lease arrangement could also be used as an alternative to the in lieu payment method. Under this approach parking expansion would be financed by tax increments or assessment bonds further secured by a pledge of annual parking lease payment to the District by new uses in the area. The payments established under either of these methods should be based on cost of providing multi-level rather than open, on-grade parking lots. The cost, however, can be adjusted to reflect multi-level use and greater efficiencies in parking utilization which would permit a reduction in required parking space to a level less than that required in the zoning ordinance. The required fees should be reviewed annually and adjusted to reflect current construction and land cost price indices. Assessment for the existing Parking District 2 should be based upon the assessed property value prior to construction of the new facilities responsible for the increased parking demand.

5. Utility Undergrounding District. The State Public Utility Commission has mandated contributions to local governments by public utility companies to assist in undergrounding overhead utility lines. Under these provisions the City of Mountain View annually receives about \$112,000. By designating a portion of the City as an undergrounding district these funds can be used to eliminate overhead utilities. Present City Council policy has designated these funds, through 1985, for undergrounding of utilities along El Camino Real. However, since Castro Square has city-wide significance, consideration should be given to allocating a portion of these funds to the Revitalization Area. The entire Project Area should be designated as an undergrounding district and a ten-year program undertaken to elimi-

nate all overhead utilities in the area. Estimated cost of this program is approximately \$436,000. It is proposed that 30 percent of these costs be financed from tax increment funds and the balance from the PUC Fund.

6. Federal Housing and Community Development Funds. Under the provisions of the newly enacted Housing and Community Development Act of 1974 approximately \$2.4 million in Federal funds will be made available to the City of Mountain View over the next five years. Among the eligible uses of these funds are the following: acquisition of land for purposes of eliminating blight, beautifying urban areas, accommodating needed public facilities, or other public purposes; construction or reconstruction of public works; construction of parking facilities; relocation payments; code enforcement; and removal of architectural barriers, such as curbs, which restrict mobility and accessibility of elderly and handicapped. With only minor exceptions, the improvements included in the Revitalization Plan are eligible for funds under the 1974 Act. Consideration should therefore be given to using a portion of these funds within the Castro Square area to supplement the previously identified sources of monies. In particular, it is recommended that Federal monies be used to implement low and moderate income housing development in the Dana Plaza area and adjoining Oak Street area.

The majority of the program improvements scheduled would be financed by means of these sources. The tax increment funds combined with the improvement district funds would finance a \$2.95 million improvement; the existing park district would contribute approximately \$190,000 for improvements of present parking sites; and PUC Undergrounding Funds would provide \$305,000 for utility undergrounding projects. All new parking required would be financed by in lieu or lease payments received from the new parking district.

Funds from these sources should, however, not be considered as substitutes for funds which normally would have been expended in the area. Instead the intent of the program is to implement changes at a scale and pace which would not otherwise be possible. Thus the Castro Square area should receive funds from regular city revenue sources such as the State gasoline tax rebates and City-County or Thoroughfare Funds. Additionally, other State and Federal funds should be sought to assist development in the area. Among these funds are State monies for pedestrian and bicycle facilities (SB 36) and the Department of Transportation's Federal Aid to Urban Areas grants. Funds are also available from the Federal Urban Mass Transportation Administration for advance land acquisition in the regional



transit station area if plans are adopted to implement a transit facility.

## **SCHEDULE OF IMPROVEMENTS**

A schedule of improvements is shown in Table 1. Also shown are estimated costs, sources of financing and phases of development. Two development phases are specified. The first, from 1975 through 1978, would concentrate on major improvement along Castro Street and the perimeter road system, excluding Villa Street, and the securing of land needed for the mini-plazas and Dana Plaza area parking and cultural facilities. Additionally, funds are allocated to initiate improvement of existing parking areas. The second phase would be undertaken from 1979 through 1982 and would include development of the mini-plazas, landscaping of areas at the rear of Castro Street businesses, improvement of Villa Street, continuation of the parking lot improvement program and improvement in the residential portions of the project. The purpose of the two-phase program is to permit time to adjust to development actions which might be stimulated by the initial program improvements and to permit coordination with plans of other agencies. Thus parking lot improvements which might conflict with the need to expand parking by construction of parking structures should be deferred and improvements along Villa Street, which are closely related to possible regional transit construction, also set off until regional transportation plans are firm.

In addition to the Phase 1 and 2 funds approximately \$970,000 is set aside to cover the following items.

1. Land Banking. City acquisition and assemblage of land is proposed in the Dana Plaza area in order to reserve land which will be needed for future parking but for which in lieu fees are not yet available; for securing of land needed for cultural facilities, and to assist non-profit or limited profit housing corporations in obtaining and assembling parcels suitable for well-designated multiple housing. It is proposed that monies used for parking acquisition be fully recovered from in lieu parking fees. In the other two applications, however, the City Council may wish to eventually apply these funds to assist either the housing-cultural facilities program or use these monies as a long-term contingency fund.

2. Dana Plaza Development Fund. The Dana Plaza cultural facilities represent a land use option which is dependent upon further resolution of the City's plans for cultural facilities. The intent is to set aside sufficient funds to assist in implement-

ing this program if Castro Square is selected as an appropriate site for these uses. Table 1 shows allocation of \$530,000 for these purposes, assuming availability of PUC undergrounding funds for Project Area improvements. If these PUC funds are not made available it would be necessary to reduce the Dana Plaza fund to \$225,000 and reallocate \$305,000 to the undergrounding program. However, if tax increments increase beyond the present level the Council may wish to allocate these additional revenues to the Dana Plaza area. The purposes proposed for these funds are to assist in financing the cost of plaza construction or construction of cultural facilities above proposed parking areas. This planned construction thereby guarantees better land utilization, more compact development and an improved visual setting.

3. Relocation Funds. With full implementation of the Castro Square Revitalization Plan, including new housing construction in the Dana Plaza area, a maximum of 12 business firms and 34 residences would be displaced over an 8 year period as a direct result of the program improvements. Under State provisions these displaced firms and households are entitled to relocation payments to cover such expenses as moving costs, loss of personal property that is no longer suitable for use at the replacement site and replacement housing payment to cover increases in interest mortgage payments which result from relocation, or the difference in the purchase price of the replacement housing and the sale of the acquired dwelling.

It is impossible at this time to precisely estimate these relocation costs since numerous factors are involved including type of tenancy (i.e. renter or home owner), length of tenancy, availability of replacement housing, and acquisition cost. In the latter instance it is anticipated that the acquisition cost of residential properties will be in excess of the cost of replacement housing since the value of the land, most of which is currently assessed based on either a commercial potential or much higher density level, represents the major portion of the acquisition cost. Thus, differential housing cost payments are expected to be minimal. A budget of \$140,000 has been set aside for these purposes.

4. Contingency Fund. Finally \$100,000 is budgeted to cover specific expenditures which cannot be anticipated, to allow for flexibility in the design and construction of facilities and to make allowances for construction cost increases. Since the program outlined in Table 1 is based upon the present level of tax increment revenues, additional tax revenues can be anticipated by the second phase of the program. If needed, these revenues can also serve as a contingency fund.

	PROGRAM ITEM	ESTIMATED COST	SOURCES OF FINANCING*				
			RF	PD2	PD3	MR	UF
PHASE 1	CASTRO STREET	\$530,000	X				
	FRANKLIN, HOPE & CALIFORNIA STREETS						
	ROADWAY RESURFACING	82,000				X	
	UNDERGROUNDING OF UTILITIES	161,000	30%				70%
	WALKWAYS, LIGHTING, LANDSCAPING	200,000	X				
	DANA STREET (BRYANT TO HOPE STREETS)						
	UNDERGROUNDING OF UTILITIES	23,100	30%				70%
	STREET RECONSTRUCTION	19,200	X				
	LANDSCAPING	82,700	X				
	BRYANT STREET (VILLA TO CALIFORNIA)						
	UNDERGROUNDING OF UTILITIES	52,500	30%				70%
	INTERIM LANDSCAPING	12,000	X				
	PARKING LOTS NO. 3, 10, and 11						
	LAND ACQUISITION	610,000	40%		60%		
	SITE IMPROVEMENTS	267,500			X		
PHASE 2	PARKING LOT NO. 9						
	SITE DEVELOPMENT	50,000		X			
	PARKING LOTS NO. 1, 4, and 8						
	SITE UPGRADING	84,000		X			
	DIRECTIONAL SIGNS	16,000	X				
	MINI -PLAZAS						
	LAND ACQUISITION	254,500 <sup>1</sup>	X				
	MINI-PLAZAS						
	SITE IMPROVEMENTS	\$206,000	X				
	REAR FACADES - CASTRO STREET	270,000	X				
	DANA STREET (HOPE TO VIEW AND BAILEY TO BRYANT)						
	ROADWAY RESURFACING	13,000				X	
	WALKWAYS, LIGHTING, LANDSCAPING	88,000	X				
	UNDERGROUNDING OF UTILITIES	29,800	30%				70%
OTHER	PARKING LOTS NO. 5, 6, 2, and 3.						
	SITE UPGRADING	56,000		X			
	OAK STREET (VILLA TO CALIFORNIA)						
	UNDERGROUNDING OF UTILITIES	50,400	30%				70%
	LANDSCAPING	12,800	X				
	VIEW STREET (MERCY TO EVELYN)						
	UNDERGROUNDING OF UTILITIES	50,400	30%				70%
	LANDSCAPING	12,800	X				
	VILLA STREET (VIEW TO BAILEY)						
	UNDERGROUNDING OF UTILITIES	68,258	30%				70%
	ROADWAY REPAVING	28,000				X	
	WALKWAYS, LIGHTING, LANDSCAPING	64,500	X				
	LAND BANK REVOLVING FUND	200,000 <sup>1</sup>	X				
	RELOCATION FUND	140,000	60%	40%			
	DANA PLAZA IMPROVEMENT FUND	530,000 <sup>2</sup> (225,000)	X				
	CONTINGENCY FUND	100,000	X				

<sup>1</sup> Includes estimated \$100,000 from resale of surplus lands acquired in the mini-plaza program.

<sup>2</sup> Monies available if PUC funds are not allocated to Castro Square and tax increments remain at the 1974 level.

\* RF - Revitalization Funds

PD2 - Funds from Parking District No. 2

PD3 - Funds from recommended parking deficiency district

MR - Funds from regular municipal revenue sources

UF - Monies from the PUC utility undergrounding fund





## REVITALIZATION PROCEDURES

**REAL PROPERTY ACQUISITION.** Implementation of the Revitalization Plan will necessitate acquisition of land and buildings now in private ownership for purposes of providing public open space, parking and other public facilities. In order to achieve the housing objectives of the Plan it may also be necessary to assemble land into sites suitable for proper development. To achieve these ends the Revitalization Agency may acquire by gift, purchase, exchange, condemnation or any other lawful methods real property in the Castro Square Revitalization Plan Area. The Revitalization Agency may use the power of eminent domain to acquire those real properties in the Project Area which cannot be acquired by other lawful means. The Agency shall not acquire, through use of eminent domain, real property which conforms with the provisions of the Plan.

**OPPORTUNITY TO RELOCATE IN THE PROJECT AREA.** It is the policy of the Revitalization Agency to encourage the participation of property owners, residents and businesses within the Project Area. In the event that residents or businesses are displaced as a result of Agency activities within the Project Area, the displaced households or firms shall be given preference in securing other land or building space in the area. The following provisions shall apply:

1. All land disposition documents in the project shall contain a provision indicating the Agency's preference for dislocated businesses and require the redeveloper to extend reasonable preferences to dislocated businesses which desire to remain or return to the Project Area, provided that the relocated business is in conformity with the requirements of the Plan.
2. At such time as third party redevelopers are in a position to seek business tenants for new development within the project, the Agency shall cause notice of such space to be mailed to all businesses relocated from the project and shall offer reasonable non-financial assistance to such businesses as may be necessary to help secure desired relocation.
3. To foster owner participation in the project, the Agency shall advise all present businesses within the Project Area of their right to compete as redevelopers of the Project Area.

**PROPERTY MANAGEMENT.** Property acquired by the Revitalization Agency in the Project Area shall be under the management and control of the Agency during its ownership of such property. Such property may be rented or leased by the Agency pending its conveyance.

**PROPERTY DISPOSITION.** For the purposes of this Plan, the Agency is authorized to sell, lease, exchange, subdivide, transfer, design, pledge, encumber by mortgage or deed of trust, or otherwise dispose of any interest in real property.

To the extent permitted by law, the Agency is authorized to dispose of real property by negotiated leases or sales without public bidding. All real property acquired by the Agency in the Project Area shall be sold or leased for development for the uses permitted in the Plan. Real property may be conveyed by the Agency to the City or any other public body without charge. Property containing buildings or structures rehabilitated by the Agency shall be offered for resale within one year after completion of rehabilitation or an annual report concerning such property shall be published by the Agency as required by law.

The Agency shall reserve such powers and controls in the disposition and development document as may be necessary to prevent transfer, retention, or use of property for speculation purposes and to insure that development is carried out pursuant to this Plan.

All purchasers or lessees of property within the Project Area shall be required to use their property for the purposes designated in this Plan, to begin and complete development of the property within a period of time which the Agency fixes as reasonable, and to comply with other conditions which the Agency deems necessary to carry out the purposes of this Plan.

**REDEVELOPER'S OBLIGATIONS.** In order to provide adequate safeguards that the process of redevelopment will be carried out pursuant to the Revitalization Plan, agreements for the disposition of land by the Revitalization Agency and owner participation agreements shall include recognizing and requiring that:

1. The purchase of land is for redevelopment and not for speculation and reserving to the Agency such powers and controls as may be necessary to prevent transfer, retention or use of the property for speculation purposes.



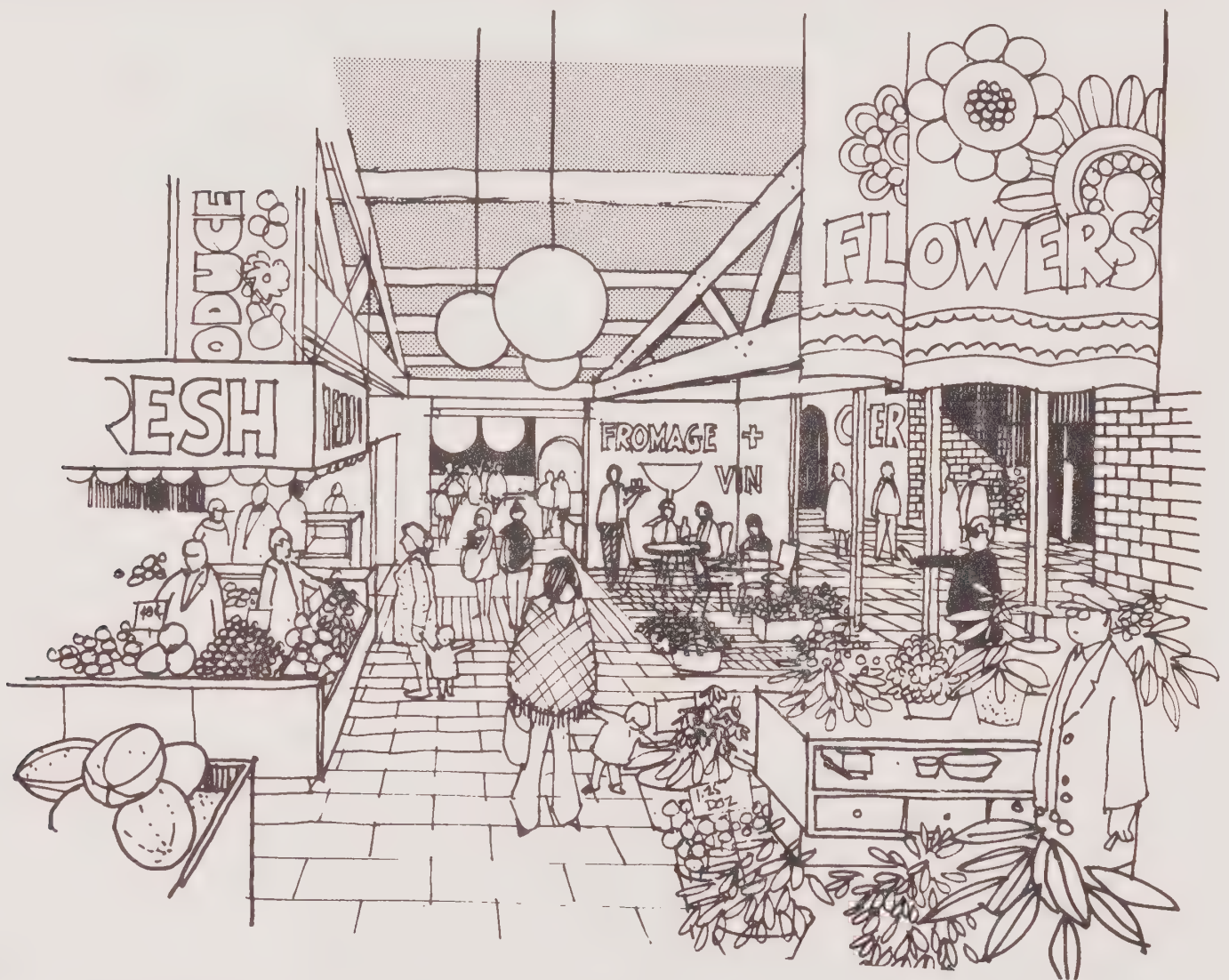
2. The land shall be built upon and/or improved in conformity with the development standards of the Plan.

3. All developers and owner participants shall submit preliminary architectural plans, site and landscape plans and final plans including landscaping and sign plans, and specifications of the improvements proposed to be constructed on the land for architectural review and approval by the Agency in order to insure that development and construction will be carried out in a manner which will effectuate the purposes of the Plan. As a part of such plans and specifications developers and, if required by the Agency, owner participants shall submit time schedules for the commencement and completion of such improvements. All such plans and schedules shall be submitted within the time specified in the respective agreements with such developers and owner participants.

4. The redeveloper or redevelopers of project land shall not sell, lease, or otherwise transfer such land at any time prior to the completion of the redevelopment thereof without the prior written consent of the Agency.

5. No building shall be constructed over an easement in the Project Area without prior written consent of the utility company concerned and the Agency.

6. By and for the contracting parties, their heirs, executors, administrators, and assigns, there shall be no discrimination against or segregation of any person or group of persons on account of race, creed, color, national origin, or ancestry in the sale, lease, sublease, transfer, use, occupancy, tenure, or enjoyment of the premises therein described, nor shall the contracting parties, or any person claiming under or through them establish or permit any such practice or practices of discrimination or segregation with reference to the selection, location, number, use, or occupancy of tenants, lessees, sublessees, or vendees in the premises described. All deeds, leases, or contracts for the sale, lease, sublease, or other transfer of any land shall contain the non-discrimination and nonsegregation clauses specified in Section 33436 of the California Redevelopment Law.



### ILLUSTRATIVE SKETCH

Uses which build upon Castro Square's established role as a restaurant and specialty food center should be encouraged.

## IV. ASSESSMENT OF IMPACTS

Overall the impacts which would be generated by the revitalization actions would be in the public benefit since the program is designed to enhance business opportunities - and in turn, increase tax revenues for the City and other taxing entities, to improve the visual appearance of this central area which is of community wide significance, to expand opportunities for community use of the area and its private and public facilities and open spaces, and to provide housing to meet the needs of low and moderate income residents as well as other residents seeking housing opportunities convenient to good public transportation. Nevertheless, any program of this scope is not without adverse impacts. The discussion below is intended, therefore, to provide a documentation of both the beneficial and adverse affects which might result from the revitalization actions.

### ECONOMIC IMPACTS

The programs outlined in the Plan will serve to stimulate increased business activities for firms now in the area as well as attract new businesses to Castro Square. The primary actions expected to produce expanded economic activity are changes in the circulation system which will favor pedestrians and transit vehicles along Castro, and improve auto circulation on the perimeter roads around Castro Square, expansion of auto parking, upgrading of the physical appearance of the area and eliminating incompatible uses which tend to detract from the area's viability.

These actions could result in the displacement of 14 business establishments consisting of a card room, liquor store, fraternal club, tire shop, stationery store, printing shop, parcel service, auto body shop and related auto storage yard, two medical office facilities, a warehouse, business machine outlet, a taxi cab facility and a small Greyhound terminal. Displacement is scheduled to extend over a five to eight year period. With the exception of the automotive related uses, opportunities would be available for these displaced firms to relocate within the Project Area. At present there is about 250,000 square feet of vacant commercial space in the Project Area. Relocation assistance and costs would be financed as a part of the Project's cost.



In addition to the direct impacts, secondary displacement of other firms might occur as a result of increased property value brought about by revitalization actions. At present there are a number of firms whose primary reason for locating in the Project Area is low rent. Many of these are on a month-by-month rental or have short term leases. It is anticipated they would seek space elsewhere rather than pay higher rentals. (It should be noted here that the intent of the State legislation authorizing revitalization programs such as Castro Square's is to promote higher land utilization. This is the sole justification for the use of tax increment financing, i. e., other taxing entities forego potential tax revenues in expectation of longer term revenues gains produced by induced investment and the resulting increase in both land and property values).

The proposed changes in the Castro Street right-of-way will decrease the auto capacity of the street between Evelyn and California Streets to about 8500 auto trips per day, approximately a decrease of 8,200 from the daily volumes carried by four lanes in 1973. However, the total number of persons traveling along Castro Street is expected to equal or surpass the present number due to county transit vehicles operating along the street. It is difficult to determine what affect these changes in type of travel will have on business activities in the area. Analysis by Gruen Gruen + Associates, undertaken during preparation of the Plan, suggest that a decrease in auto traffic along Castro Street would not adversely affect overall business activity. In response to the question "would you shop with less or greater frequency on Castro Street if Castro Street were closed off between California Street and Evelyn Avenue so that you were not able to park along Castro Street, but would have access to shops from Hope or Bryant Street," 77 percent replied they would shop with the same frequency, 16 percent said they would shop less frequently and seven percent indicated they would shop more frequently.

## **SOCIAL IMPACTS**

The revitalization program provides a means of implementing two major social goals of the City; namely creation of Castro Square as a viable social and cultural focus of the community and provision of housing to meet the needs of low and moderate income residents and others seeking accommodations other than single family detached homes.

However, in order to achieve these social objectives, as well as other objectives of the Plan, short term adverse impacts for some existing residents of the Project can be expected. The severity of the impacts is expected to be minimal if the housing programs outlined in the Plan are implemented and other mitigation steps discussed below are followed. Briefly, up to 34 housing units could be demolished as a result of actions programmed in the Plan. Land proposed for expansion of parking and civic facilities along Bryant Street would remove seven housing units and residential development proposed as part of the Dana Plaza development would remove an additional 27 units. With the exception of several multiple units the housing stock to be displaced is composed of small, single family detached structures built 40 or more years ago. The area within which they are located is characterized by an incompatible mix of residential and commercial uses. Moreover, the majority of the area is presently zoned for commercial purposes, a factor which has contributed to prevalence of substandard housing conditions and which would inevitably lead to demolition of these units through normal market processes.

Displacement of residents in these units would extend over the eight year period shown in the Improvement Schedule and possibly beyond that period. Thus the number of households to be relocated per year would be low.

There exist three potential housing relocation sources within the immediate area.

1. City owned property acquired initially for public purposes but which might become surplus. Potential areas include a portion of the properties acquired for civic center expansion between Franklin Street and Bailey Avenue adjoining the Civic Center, land acquired for the Calderon overcrossing and widening of Calderon Avenue, and land acquired east of View Street for Castro Square related parking. The status of these sites will depend on future City Council actions.
2. The Dana Plaza area provides opportunities for construction of upward of 200 new units of housing. Since a portion of the proposed housing area includes present City police and fire facilities proposed for relocation, housing could be provided here during initial phases of the housing program so as to provide replacement housing in advance of actual displacement. Similarly, a portion of the area slated for housing is now devoted to commercial uses which are proposed to be eliminated. This land supply could also be scheduled for early housing construction.
3. There are scattered vacant lots in adjoining areas to which structures in the Project Area might be moved. The main problem encountered with this source is

the generally poor condition of the residential structures which would be moved.

The greatest problem is created for renters in the area since rents tend to be low and the renters have low or moderate incomes. Provisions of the Housing and Community Development Act of 1974 should be used to provide rent supplements for these residents if needed. The problems of homeowners is not anticipated to be as severe since land acquisition cost will be based on market cost reflecting higher utilization than the modest single family structures now occupying sites here. Consequently, the present homeowners should be able to afford suitable replacement housing in other areas of the city or in the Project Area itself.

Since other City projects in the immediate area will also create housing relocation problems it is proposed that the residents in seven units to be displaced by land acquisition for parking in Phase I of the improvement program be provided for by use of other City owned residential property. The balance of the relocation needs should be provided for by construction of new housing within the Dana Plaza area on land not now occupied by residential uses. New housing constructed here could also assist in meeting relocation needs for other City projects.

## **TRAFFIC IMPACTS**

Major modifications made in the existing circulation pattern consist of elimination of two travel lanes along Castro Street in order to expand pedestrian accommodations, and provide service by public transit, modification of the present two way traffic flow on Franklin and Hope Streets to one-way movements, conversion of Dana Street between Bryant and Hope Street to a one-way street with diagonal parking -- and in the longer term future conversion to pedestrian use, expansion of parking facilities along Bryant Street between Villa and California Streets, and allowances for a regional and/or county mass transit station along the Southern Pacific Railroad right-of-way between Castro and Franklin Streets. Other provisions include a grade-separated pedestrian and bicycle crossing of the Central Expressway and Southern Pacific railroad track and other special accommodations for bicycles and pedestrians. The impacts of these major proposals are discussed below.



CASTRO STREET MODIFICATIONS. The proposed changes along Castro Street would restrict traffic movements along Castro Street. Anticipated results are as follows.

1. Traffic would be reduced from the early 1974 level of about 16,700 vehicles per day (between Evelyn Avenue and California Street) to about 8,500 vehicles per day. Even with restriction of left-turn movements at Dana Street, free-flowing traffic conditions cannot be expected.

2. The modifications to Castro Street would increase traffic along Villa and Franklin Streets and a portion of View Street from Evelyn to Villa Street. Franklin Street is expected to increase by 3,000 vehicles, leading to a total volume of 4,000 vehicles per day. This level is considered consistent with land uses proposed along Franklin Street. Due to the restriction of left turns onto Castro Street from Evelyn Street east of Castro Street and elimination of movements between the portions of Evelyn Avenue east and west of Castro Street, traffic volumes would increase on Villa Street and on View Street between Evelyn Avenue and Villa Street. No major adverse impacts are anticipated since the increase in traffic volumes is still within the streets' capacity and the adjoining frontages are in commercial use. Hope Street should continue to carry traffic on the order of current volumes -- less than 6,000 vehicles per day. Current southbound traffic would be replaced with an approximately equal increase in northbound traffic.

3. Approximately two-thirds of the auto trips on Castro Street are through trips. Thus the Plan would impose some inconvenience on motorist traveling through the area in order to achieve other community objectives. The proposed completion of Bailey-Central Expressway Interchange, provision of a Central Expressway-Stevens Creek Freeway connection and construction of a Moffett and Bailey Avenue connection would serve to accommodate these diverted trips. Moreover, operation of local and county transit service should also divert many of these through trips, which tend to be work related travel, from auto to transit thereby reducing the total number of auto trips to be accommodated in the Castro Square area.

4. The proposed improvements would eliminate about 50 parking spaces along Castro Street. This loss would be compensated by expansion of parking on Dana Street plus major parking additions along Bryant Street. The county and local transit service, which is partially responsible for elimination of the curbside parking spaces, will increase assessability to business properties along Castro Street. Moreover, all business properties along Castro Street have direct public

auto access and public parking provided from Bryant and Hope Streets. Therefore, no major adverse impacts on business activity is anticipated.

**CASTRO STREET-EVELYN AVENUE INTERSECTION.** The Plan makes provisions for the continuation of the present auto movements between Evelyn Avenue east of Castro Street and the Central Expressway and Moffett Boulevard. At present Castro Street has daily traffic volumes of about 22,000 vehicles between Evelyn Avenue and the Central Expressway. With continuation of the present at-grade crossing of Castro Street with the Central Expressway and the Southern Pacific Railroad tracks any future growth in traffic will add to the congestion which already negatively affects the accessibility and functioning of the Castro Square area.

**PUBLIC TRANSIT IMPROVEMENTS.** Provisions for regional, county and local transit, if coordinated with each other and with land use development in the Castro Square, would serve to reduce the dependency on the private auto for both work related travel and non-work travel such as shopping. The placement of the regional transit station, although functional, contributes to traffic conflicts which could be eliminated by locating the station astride Castro Street. This would improve access by transit operating on the surface streets, particularly Castro Street, and reduce movements across Castro Square from residential areas to the east by providing for transit parking east of Castro Street.

**BICYCLE CIRCULATION.** The Plan provides for grade-separated bicycle access to Castro Square from areas north of the Central Expressway via a new non-vehicular overpass near Castro Street. This route would replace the existing bikeway provided on the Bailey overpass structure which should be discontinued because of potential safety problems if a ramp from Bailey Avenue to the east-bound lanes of the Central Expressway is constructed. Separate bicycle accommodations are also proposed along Bryant and Hope Streets. Overall the effect should be greater accessibility to Castro Square and reduction of hazards to bicyclists.

**PEDESTRIAN CIRCULATION.** Priority is given to pedestrian movements along Castro Street, from Evelyn Avenue to California Street, and along Dana Street, Bryant Street -- between Villa and California Streets, and Mercy Street -- be-

tween Franklin and Castro Streets. Additionally, pedestrian access to Castro Square would be provided from areas to the north of the Expressway via a non-vehicular overpass of the Expressway near Castro Street. All of these improvements would improve pedestrian access from adjoining neighborhoods and reduce pedestrian safety problems, particularly at the Expressway and Southern Pacific right-of-way and along Castro Street.

## **ENVIRONMENTAL IMPACTS**

If the Plan is implemented it would produce substantial improvements in the visual setting of Castro Square. Overhead utility lines would be undergrounded, roads and parking areas throughout the area landscaped, the appearance of the street spaces improved by elimination of chaotic signing, and visual relief provided by the system of mini-plazas. Moreover, through the administration of the specific plan provisions incremental improvements in building design would also be achieved.

Efforts to improve air quality would also be assisted by this program by provision for local and county transit operation along Castro Street and by facilitating coordination of county, local and regional transit service. Additionally, by promoting compact commercial land use close to local, county and regional transit accommodations greater use of public transit for local work and non-work related trips would be encouraged. Similarly, construction of multiple housing, with the parking restriction proposed, would help promote greater use of public transit.

The proposed circulation system, which would eliminate a direct connection between California Street and Dana Street east of Castro Street, would protect the neighborhood to the east of View Street from noise intrusion and traffic safety problems produced by through automobile movements. The proposed redesign of Dana Street, between Bailey Avenue and Franklin Street would also serve to promote an environment suitable for residential purposes in the area bounded by Bailey Avenue, Villa, Franklin and California Streets.



## FISCAL IMPACTS

The Castro Square program would be financed largely from tax increments, i. e., increases in tax revenues produced by development since declaration of the area as Project Area under the provisions of the State Community Redevelopment Act. The rationale underlying the State's authorization of tax increment financing is that the revitalization actions will promote a higher level of land utilization than would be possible without use of redevelopment powers. Thus, tax revenues which would have otherwise continued to go to other taxing entities such as the County, the school district and other special districts are temporarily diverted to help finance Project Area improvements in expectation that the increase in the tax base produced by the improvements will, in the long run, produce total revenue for these entities greater than would have otherwise been possible.

In the Castro Square project tax revenues of other taxing agencies have been diverted since creation of the Project Area in December of 1969. The bonding program set forth in the Plan calls for issuing 15 year term bonds. Assuming improvements made are equal to those which can be financed by tax revenues already accrued and by tax increments produced by the existing level of investment in the area -- and supplemented by the other sources of funding noted in the Plan--the project must produce sufficient increases in investment to offset the diversion of approximately \$3.3 million in tax revenues from other agencies and the financial loss they experience by deferring these revenues until a future date. The project will have an adverse fiscal impact on these other governmental entities if private investment attracted to the area is not sufficient to make up this loss. Other fiscal costs include administrative cost produced by the project. Although the Plan has been drafted to simplify administration through use of specific plan procedures and by maintaining, where possible, consistency between these provisions and the provisions of the City's present zoning ordinance increased administration time can be expected. The public improvement programs specified will require city staff coordination and these in turn will produce, if the program is successful, similar increases in private construction activities. This will require additional city staff time. Tax revenues produced by the new private investments would however, offset the added city staff costs.

The Plan's allowances for the continuation of the at-grade crossing of Castro Street and the Southern Pacific Railroad tracks also has fiscal implications. In

order to receive State Public Utility Commission funds for construction of a grade-separated crossing of the railroad in central Mountain View it is necessary to eliminate the present grade crossing to be eligible for the funds.

## **CONSISTENCY WITH THE GENERAL PLAN**

For the most part the Plan provisions are consistent with the City's current general plan and is designed to promote the aims established in that plan. There are three areas, however, where differences occur between the two plans. The first of these regards the status of Castro Street. The General Plan calls for eventual closing of Castro Street at the Central Expressway. The Revitalization Plan makes no provision for closing Castro Street. It does not, however, foreclose this choice. The second difference concerns the status of Dana Street east of Castro Street. The General Plan provides for linking of California Street with Dana Street in the vicinity of View Street. The Revitalization Plan proposes that this link not be permitted. The third difference is produced indirectly by the Revitalization Plan's proposal for continuation of Castro Street at-grade with the Southern Pacific Railroad track and the Central Expressway. This alignment would physically, and probably fiscally, preclude construction of the Calderon overcrossing proposed in the General Plan.

#### SEDWAY/COOKE PROJECT STAFF

Tom Cooke, Partner-in-charge  
Paul H. Sedway

Jack Schnitzius  
Pam-Anela Messenger  
Lynda Wagstaff  
William So  
Walter Rask  
Pat Scarlett  
Darlene Luppino  
Debbie Harrington

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The logo used on the cover was originally designed by Bobbi Reich-Patri, Graphic Designer.



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